

# Staff Report to Council

Planning and Development

FILE: 3060-20-2019-07

REPORT DATE: May 11, 2021

MEETING DATE:

May 18, 2021

TO: Mayor and Council

FROM: Anne Berry, Director of Planning and Development

SUBJECT: Development Permit Application for Golden Ears Business Park  
Phase 4

CHIEF ADMINISTRATIVE OFFICER REVIEW/APPROVAL:



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RECOMMENDATION(S): THAT Council:

- A. Approve issuance of Development Permit No. 2021-005 for Golden Ears Business Park Phase 4 at 11208 Harris Road; OR
- B. Other.

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## PURPOSE

To present the development permit application for Phase 4 of the Golden Ears Business Park to consider issuance.

☐ Information Report

☒ Decision Report

☐ Direction Report

## DISCUSSION

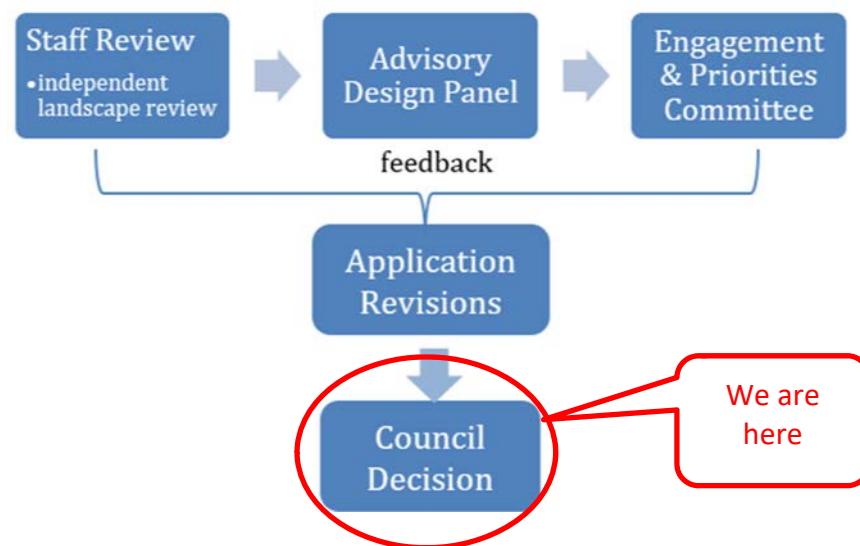
### Background:

The site was rezoned from A-1 (General Agricultural) to I-3 (Light Industrial Business Park) on May 22, 2018. A development permit application was submitted to develop the site. A development permit (DP) specifies how development can occur on a property that has been designated in an Official Community Plan (OCP) as being in a Development Permit Area (DPA). Under the City's Official Community Plan Bylaw No. 2352, 2007, Development Permit Area No. 4 Business Park applies to the properties.

A DP application is reviewed by staff and is compared to the applicable OCP DPA guidelines. Depending on the development, it may not meet all the requirements, but it must meet the guidelines' intent to the best of its ability. If there is a reasonable rationale for a divergence from the guidelines, and the change meets the guidelines' overall intent, Council can accept the non-compliance.

Local government discretion in issuing or refusing a development permit is limited to the development permit area guidelines and objectives. If an application is compliant with the DPA's applicable guidelines, the development permit should be issued. If issuance is refused, reasons for refusal should be provided.

Due to the scale and public interest in this project, Council endorsed a unique review process for the development permit application as follows:



This DP application was presented to the City's Advisory Design Panel (ADP) over four meetings to discuss the form and character aspects of the application. The applicant amended the plans to incorporate the ADP's comments.

At the March 30, 2021, Engagement & Priorities Committee Meeting, Council and members of the public provided comments and feedback to the developer on the form and character of the proposed design. Subsequently, the DP plans have been revised to incorporate feedback that aligned with the intent of the design guidelines:

- Added covered structures for outdoor employee amenity area;
- Added traffic calming measures, being speed bumps at various locations in the vehicle parking area;
- Clarified details about the loading court; and
- Structural soil for parking lot island trees added.



### **Relevant Policy, Bylaw or Legislation:**

The site is located within Development Permit Area (DPA) No. 4. The objectives of this DPA are:

- To encourage high standard industrial development that is cohesive and integrated into the surrounding environment; and,
- To create sustainable and environmentally friendly development by applying green building technology and sustainable building practices.

These DPA guidelines were developed by an independent consultant (IBI Group) and adopted by Council in April of 2019. The guidelines contain sections on streetscape, greenway, buildings, landscape, employee and community amenities, lighting, signage and parking, loading, and storage.

The DP application may not be able to meet all the guidelines but must meet the intent. An overview of the development's main characteristics follows, and the design guideline compliance checklist is appended to this report as Attachment A.

### **Analysis:**

In general, when the DPA guidelines were developed, it was envisioned they would apply to a site with several buildings. Numerous points in the guidelines refer to buildings facing a street, such as requiring extra consideration for architectural details and offices to be oriented at the front of buildings. Loading is not recommended facing a street and should be oriented inwards. Airport Way and Harris Road are designated as prominent frontages in the guidelines, where buildings should be oriented towards.

In this case, however, only one building is proposed, which makes it challenging or impossible to meet some of the guidelines in the DPA. As the development permit application progressed, based on recommendations by the Advisory Design Panel, and in discussion with the City and members of Council, the design and site layout evolved. The main entrance and front of the building are oriented towards a residential neighbourhood and not the street. This way, the entire loading court is located as far away from residential development as possible, on the west side of the building, to reduce noise levels. The prominent office space also provides a transition from residential to warehouse uses.

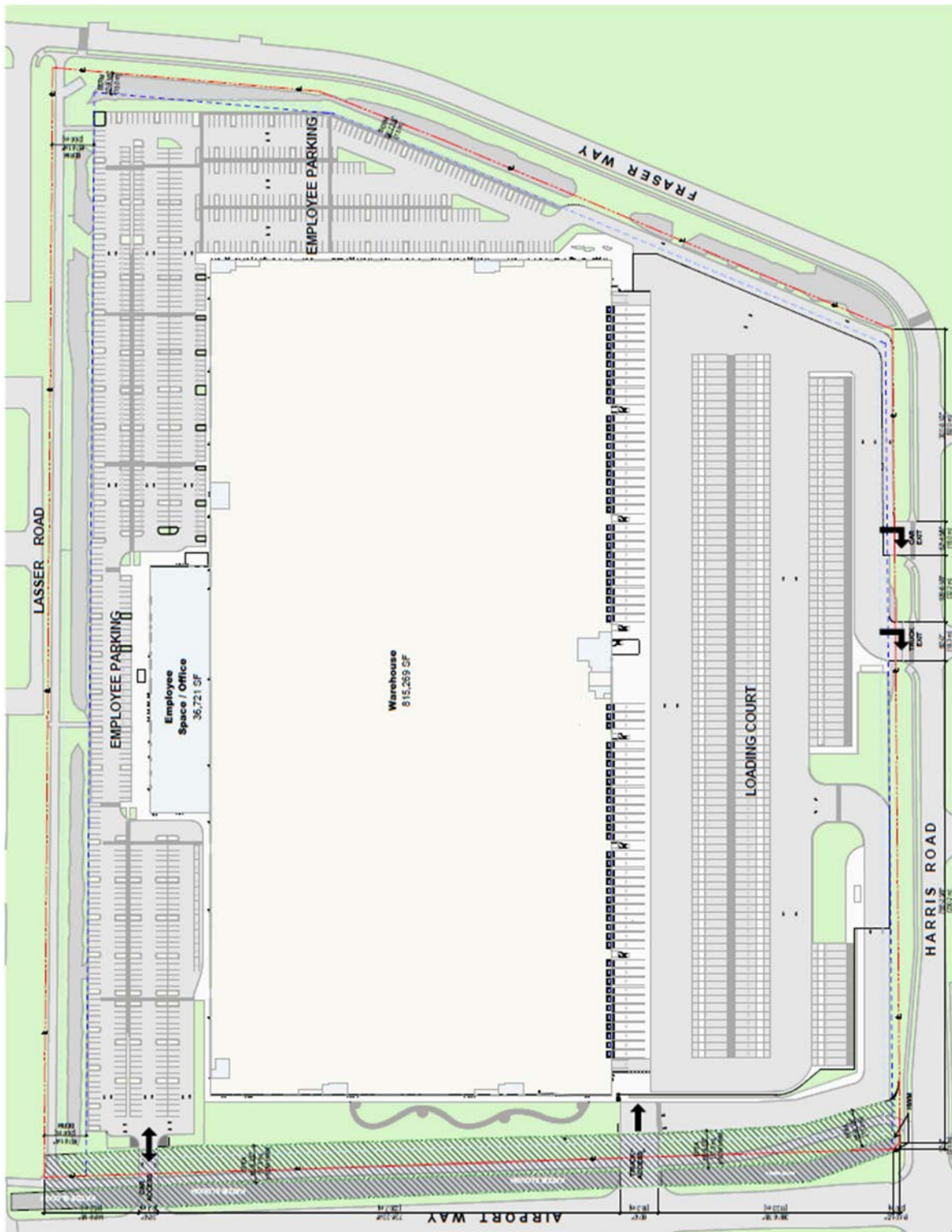


Figure 1: Overall site plan

## Building Design

As mentioned, development in Phase 4 is proposed to consist of one large building of 80,248 m<sup>2</sup> (863,785 ft<sup>2</sup>). The majority of the building occupancy will be light industrial warehouse space, with 4,177 m<sup>2</sup> (44,963 ft<sup>2</sup>) proposed for office use.

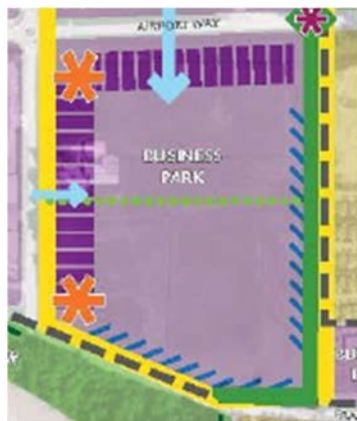
The maximum amount of floor area permitted for the site based on the I-3 zoning is 192,645 m<sup>2</sup> (2,073,613 ft<sup>2</sup>).

The building is proposed at 11.9 m in height, just under the Zoning Bylaw's 12 m. The proposed site coverage is 41.7%; there is no site coverage regulation in the I-3 zoning. As proposed, the use, size, height, lot coverage, and siting of the building conform to the Zoning Bylaw requirements.

The DPA guidelines suggest modulating building façades, differing materials and colours, strong architectural expression on corners and glazing on ground floor elevations facing a street. The building is to be constructed of tilt-up concrete panels. It proposes corner architectural treatments that are relatively extensive, including faux wood elements, glazing, natural-tone colour variations, canopies, and articulated massing, as outlined in the guidelines.

## Site Access

Only one access point to the site from both Airport Way and Harris Rd was envisioned, as identified by the blue arrows in Figure 2.



*Figure 2: Comprehensive development plan from DPA guidelines*

However, the developer has advised that this is not operationally feasible. Due to the volume of vehicles, queueing to enter or exit the site would be substantial with only one access point per road and likely lead to driver frustration and unsafe maneuvers. For this reason, multiple access points are proposed off Airport Way (two access points) and Harris Road (two egress points) and differentiated for employee/customer and commercial vehicles, as shown below in Figures 3 and 4.

A narrower driveway entrance is proposed on the east end of the site, for employee and customer vehicles, leading to employee and customer parking. These vehicles will have the option of exiting the site from that driveway (eastbound or westbound) or an exit going northbound on Harris Rd. A median and signage are planned at the Harris Rd exit to prevent vehicles from using Fraser Way.

Commercial vehicles are proposed to enter the site from Airport Way into the loading court on the west side of the building. These vehicles will exit going northbound onto Harris Road and signage will be installed informing drivers that they must go north.

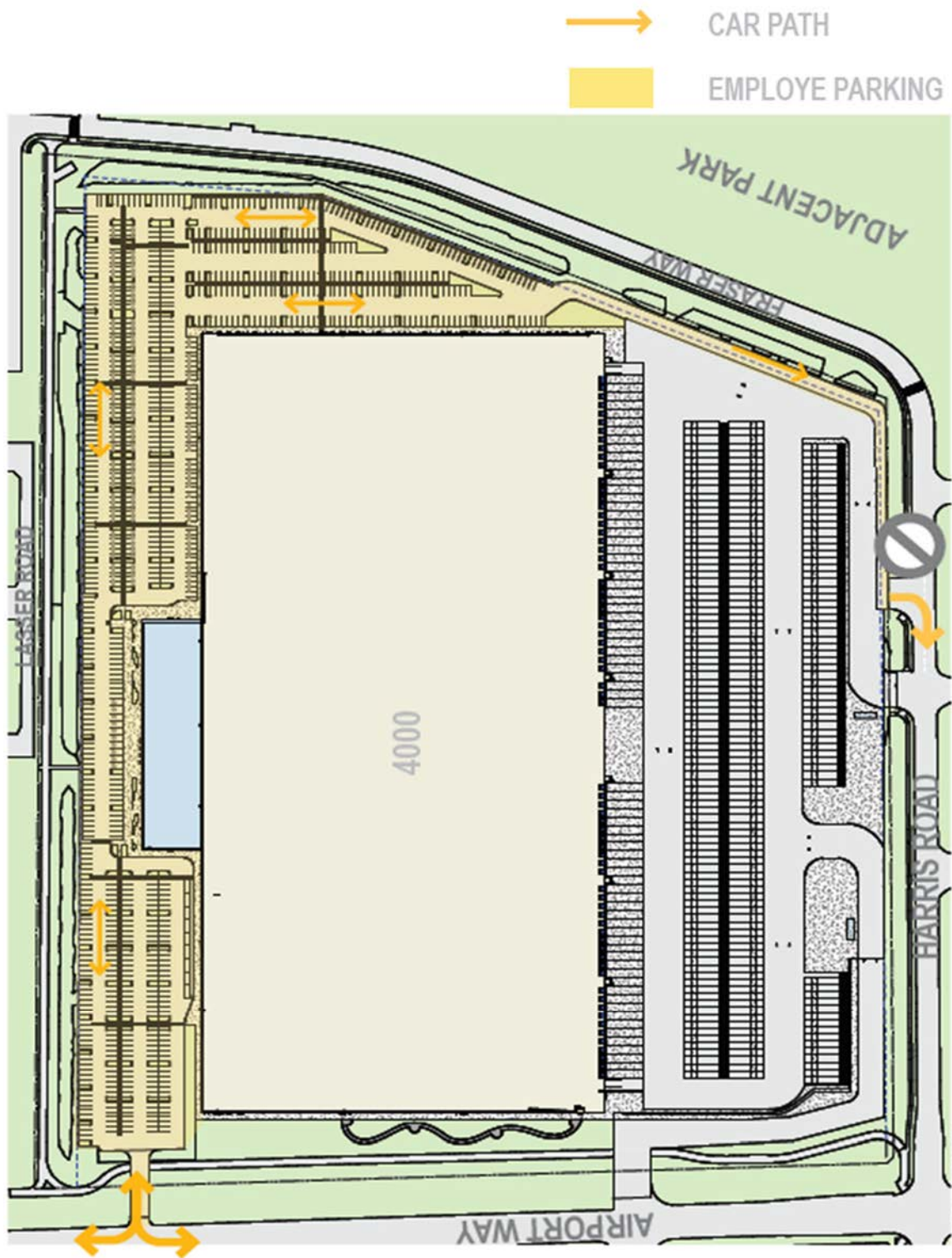


Figure 3: Employee/ customer vehicle circulation and parking



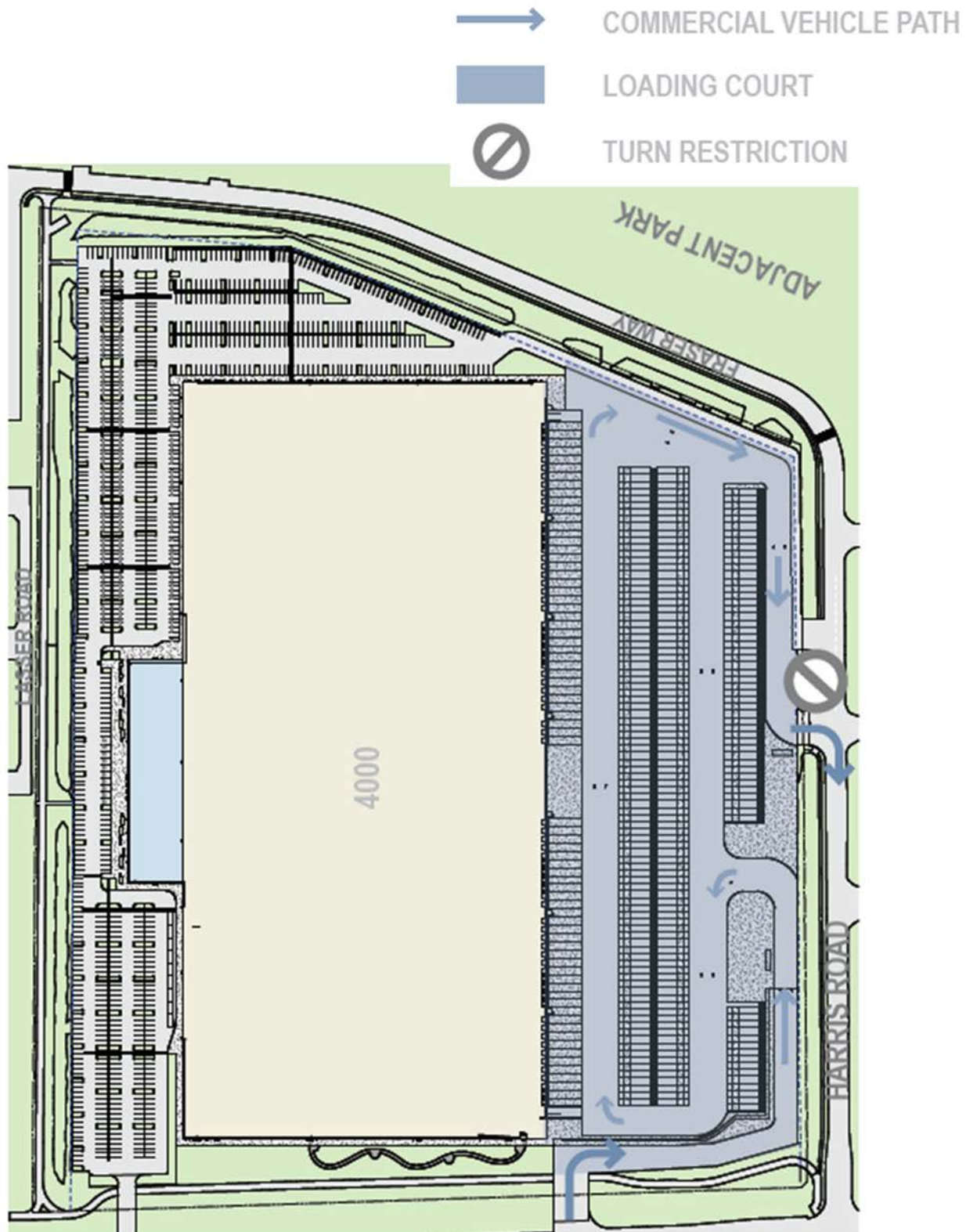


Figure 4: Commercial vehicle circulation and parking

## Traffic and Access Assessment for Phase 3 and 4

While a traffic study to this extent is not typically undertaken at the Development Permit stage, it was necessary to conduct this work to understand the impact on area traffic patterns, additional loads on intersections, proposed improvements to roads, and a rationale for the vehicle access points.

As previously mentioned, an initial traffic study was completed for the area by McElhanney in 2016, which recommended the four-lane widening of Airport Way and signalized intersections at Harris Rd and Bonson Rd. This study assumed right-in/right-out access points for GEBP Phase 3 and 4 and did not account for the sites' specific operations. CTS, Onni's traffic engineer, completed an updated traffic study to account for the nine access points shown below and associated impacts on Airport Way and Harris Rd. Similar to the 2016 McElhanney report, annual growth (2%), GEBP Phase 1 and 2, the Nature's Walk development and the potential school site were considered in the updated study. A third-party peer review was also completed by McElhanney to ensure continuity with the 2016 information and provide assurances to the City that all aspects were considered.

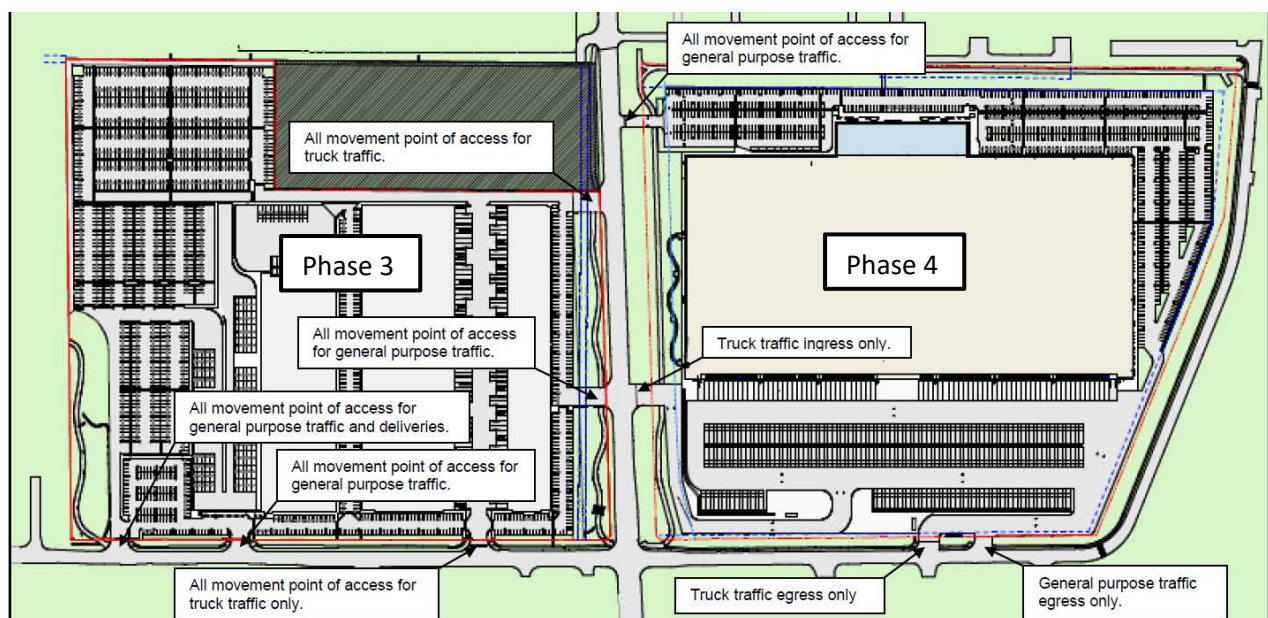


Figure 5 – Site Access Points for GEBP Phase 3 and 4

The GEBP Phase 3 and Phase 4 development is significantly smaller than the assumptions in the 2016 traffic study (1,356,907ft<sup>2</sup> over four buildings, instead of 1,867,700ft<sup>2</sup> over eight buildings assumed in the 2016 study), resulting in substantially less vehicle traffic and improved levels of service. Phase 3 and 4 together are expected to generate an additional 545 vehicles during peak hours. All assessed intersections and access points operate within acceptable levels of service. The traffic improvements will include:

- Upgrading Airport Way to four lanes between Baynes Rd and Golden Ears Way;

- Signalized intersections at Airport Way and Bonson Rd and Airport Way and Harris Rd;
- Sidewalk construction on the east side of Harris Rd from Fraser Way to Fieldstone Walk and west side of Harris Rd from Airport Way to Fieldstone Walk;
- Construction of 4 metre wide trails along Airport Way and east sides of both sites;
- Bike lanes on Bonson and Harris Roads between Fraser Way and Airport Way;
- Mid-block, at-grade, signalized pedestrian crossing on Airport Way;
- At-grade, signalized pedestrian crossing on Harris Rd, near Fieldstone Walk; and
- Two new bus shelters on Bonson Rd

The widening of Airport Way is to be constructed at 50% build-out of Phase 3 and 4, which is expected to occur before 2024. Onni has agreed to coordinate a third-party review of the roadworks operation one year after completion to ensure the performance/operations meet the design's intent. If defects are observed, the issues would be remedied at Onni's cost. This condition will be outlined in the Servicing Agreement.

Although Phases 3 and 4 together produce substantially less vehicle traffic than predicted in the 2016 McElhanney study, the roundabout at the east end of Airport Way at Golden Ears Way (GEW) will not be able to accommodate the volume of traffic expected in the next 2.5 years and significant modifications are recommended. The City has shared the study and preliminary designs for the Airport Way widening with TransLink for further discussion and necessary planning.

### **Stormwater Management**

A stormwater management plan (SWMP) is not typically mentioned during the development permit stage and is reviewed by staff before issuance of a building permit; however, staff have provided a high-level update.

There will be both an on-site and off-site SWMP for Phase 4. The culvert design for the Harris Rd (south of Airport Way) and Fraser Way ditches will be shown, and all the stormwater from on-site and within the new culverts will be directed towards the Fraser River via a separate gravity-fed outlet. The outlet will extend through the dike along Fraser Way without adverse structural impacts. Re-directing this stormwater away from the Baynes Rd pump station will alleviate some of the higher water levels observed in the area and help to counteract the additional runoff anticipated from widening Airport Way.

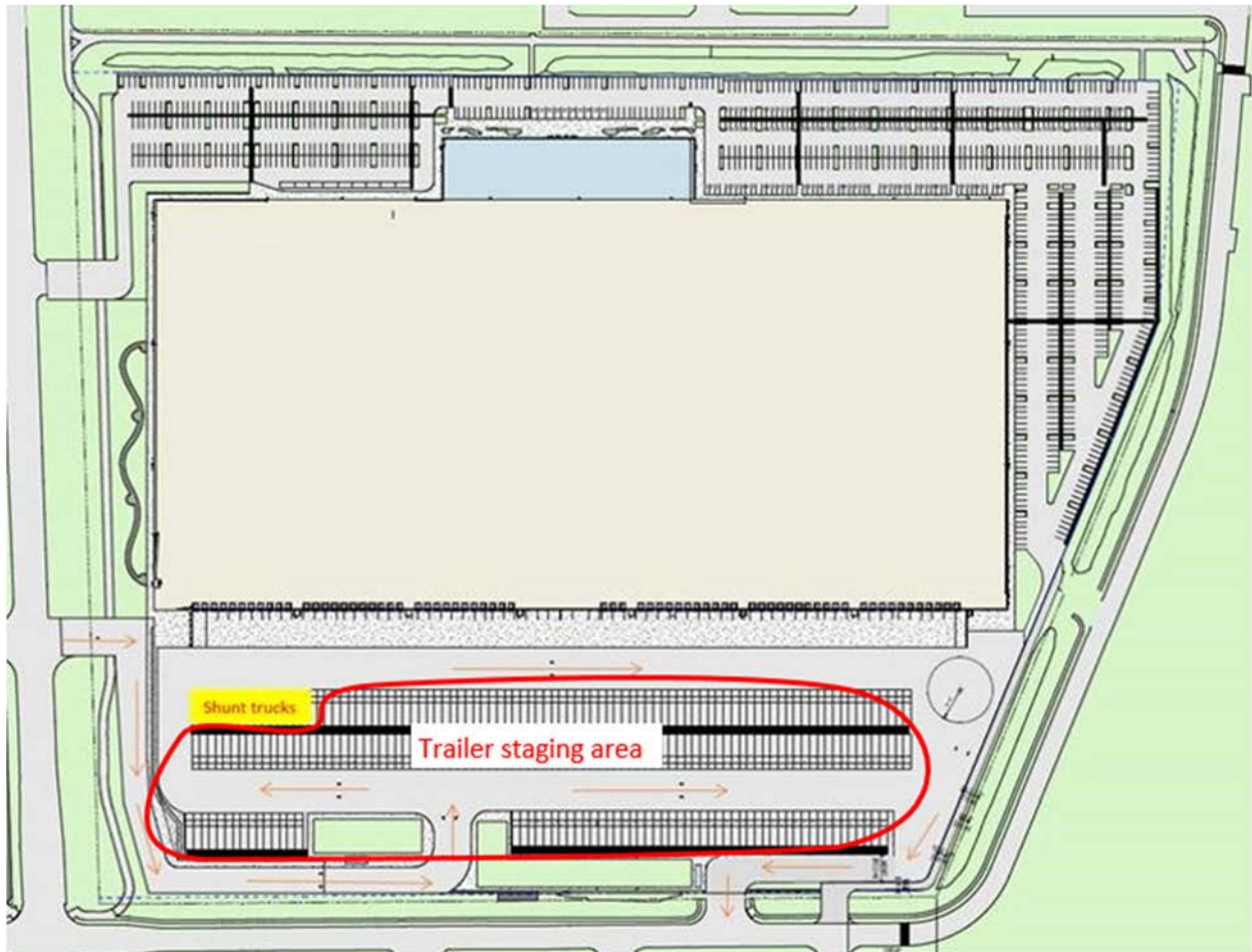
### **Parking and Loading**

A total of 915 vehicle parking spaces are proposed for employees and visitors, oriented along the east and south sides of the building. Bicycle parking spaces are also required



and will be provided by the tenant. A loading court with 319 staging/loading spots is proposed on the west side of the building, as far away from residential development as possible. The total parking spaces proposed exceeds the minimum required by the City's Zoning Bylaw (854).

In the Zoning Bylaw, loading areas are not permitted within any of the required building setback areas. For this development, the closest property line to the loading court and trailer staging area is over 20 m away, which exceeds the minimum setback requirements of 7.5 m.



*Figure 6: Trailer staging, shunt truck location and circulation*

Trailers will be staged prior to being shunted to the loading docks to be loaded or unloaded. Electric shunt trucks will move trailers from the storage queuing area to and from the loading docks. There may be some temporary storage of trailers overnight. The shunt trucks are electric and will be stored in the designated storage area which includes charging stations. Figure 6 shows the storage location for the shunt trucks and their circulation route.

As outlined in the DPA guidelines, loading is located away from residential and park areas, and separates vehicle parking from the loading area. The DPA guidelines also suggest orienting loading towards the interior of a site and away from prominent frontages (Harris Rd and Airport Way). For this site and design, compliance with both of these guidelines is not possible. Loading cannot be oriented towards the interior as there is only one building proposed and by having the loading bays face Harris Rd, they are located further away from adjacent residential areas, and thus this loading orientation can be supported.

The loading area will be gated and inaccessible to general vehicle traffic. A 6' high fence is proposed around the loading court (shown in black dashed lines) and a hedge (planted height 5', will grow to 18-20') for additional screening proposed beside the fence along the north side (circled in red dots), shown in Figure 7 below:

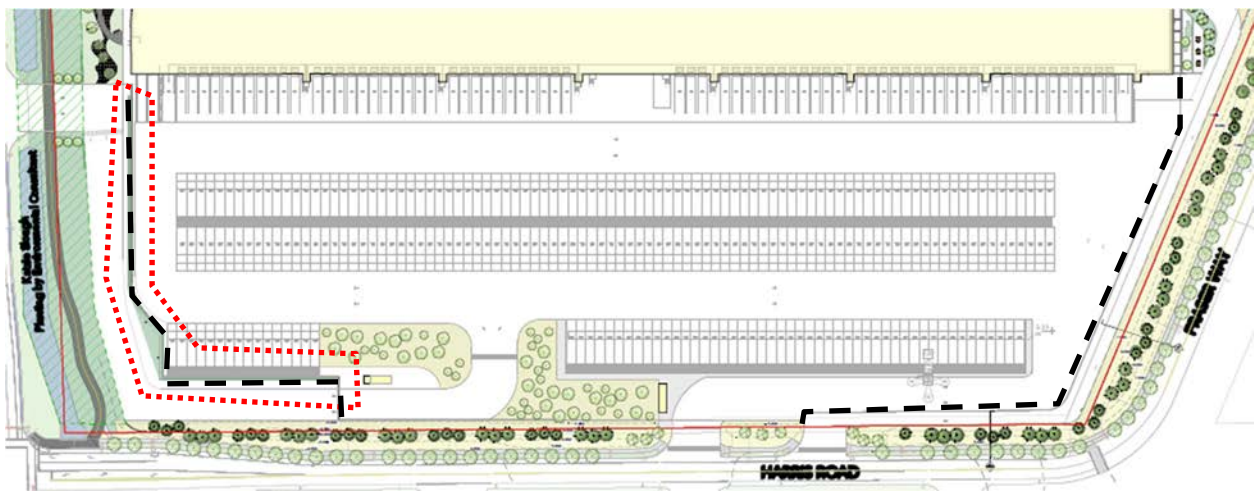


Figure 7: Fencing and screening around loading court

The south end of the loading court will be screened by the berm along Fraser Way (shown later in Figure 11).

## Sound

As noted in the previous report to Council presented at the EPC, the developer has agreed to enter into a Memorandum of Understanding (MOU) with the City which includes a follow-up acoustic study when the site is operational, should noise concerns arise. Within the MOU the developer has confirmed its commitment to future noise attenuation measures as may be necessary and appropriate in connection with the proposed development including a commitment to building an additional sound wall somewhere on the east and south property line if deemed necessary.

## Traffic Calming

As heard at the EPC Meeting, Onni is amenable to adding some traffic calming measures to the internal site to reduce off-hours speeding. Speed bumps are proposed in several locations throughout the vehicle parking area (see Figure 8).

Generally, City staff do not recommend reviewing or evaluating internal traffic flow and layouts due to potential liability exposure. A condition in the development permit is recommended to permit traffic calming measures on an as-needed basis identified by the developer. This will allow flexibility for future changes as operationally may be necessary as the site is developed and used going forward.

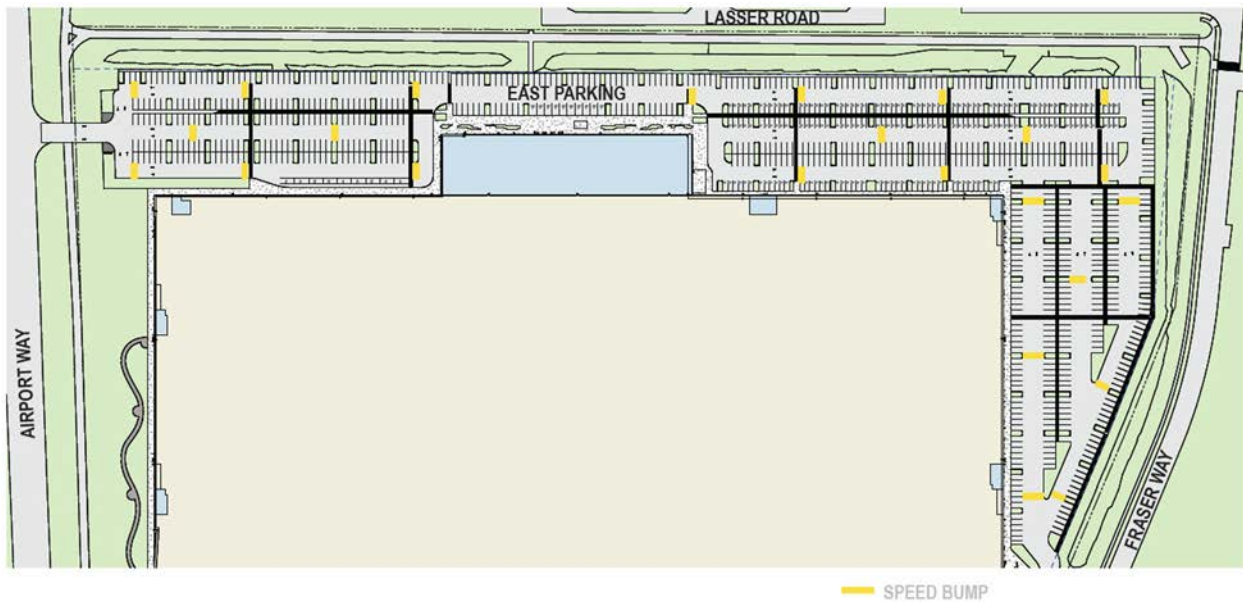
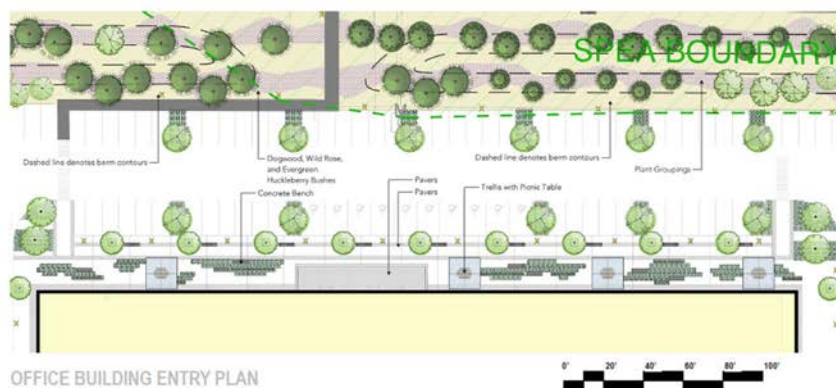


Figure 8: Speed bumps in the vehicle parking area

## Outdoor Amenity Areas

During the EPC meeting, a suggestion was made to add a cover to some of the outdoor amenity areas to be used by tenant employees. The developer is proposing to include trellis shelters (4-5) outside the main office entrance, as shown in Figure 9.



Uplift Covered Structure  
by Landscape Forms



Figure 9: Covered trellis for outdoor amenity areas

## Greenway and Landscaping

Along the site's perimeter, especially the east and south sides, green space buffers with trails and berms are required as part of a development covenant on the title. Connectivity to and from the site to city trails and sidewalks is essential.

Landscaping is critical along the perimeter, which is why an independent third-party consultant reviewed the landscape drawings. The plans were revised to incorporate all of the comments provided by that landscape review.

One tree is proposed for every sixth stall in the customer and employee parking areas as required by the DPA guidelines.

Along with the east property line is a 20 m buffer area which will contain a multi-use path and 9 ft high berm with trees planted on top of the berm.

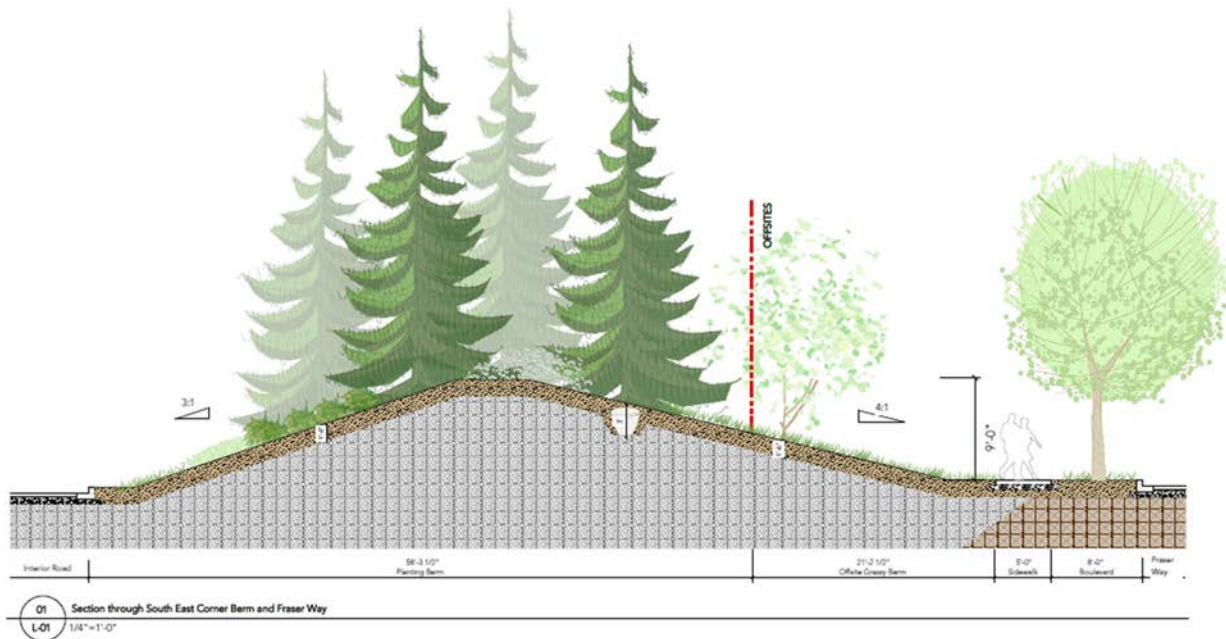


Figure 10: Section of berm along the east property line

Along the south property line is a 10 m buffer area that tapers to 7.5 m moving west, which will contain a 7-9 ft high berm. Taking into account the smaller buffer area (7.5 m versus 10 m) and the sight line considerations for traffic around the corner of Harris Road and Fraser Way, the berm is lower at the west end of Fraser Way (7') than the east end (9').

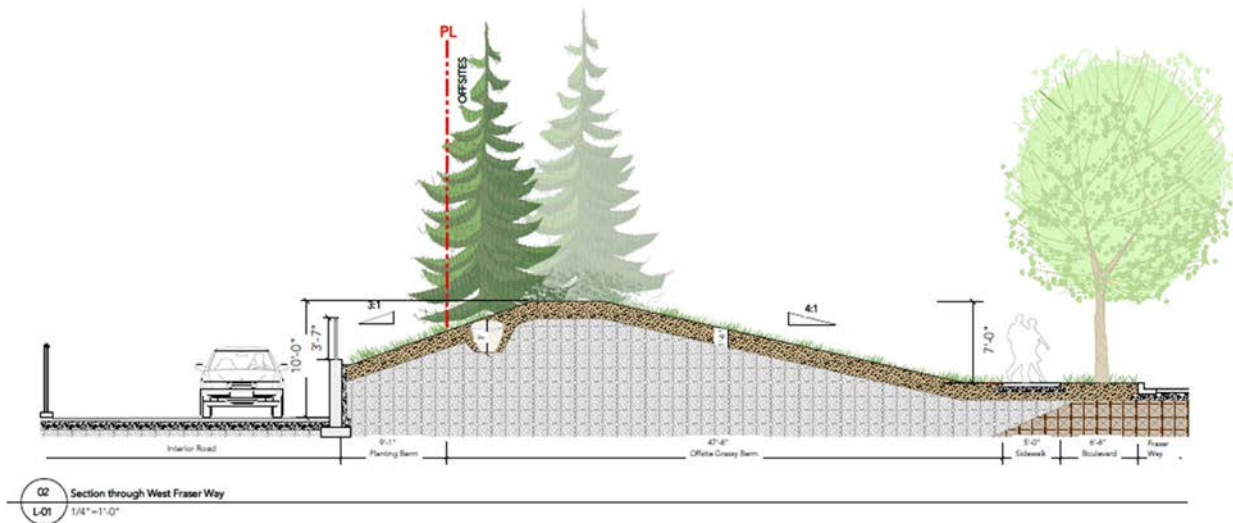


Figure 11: Section of berm along Fraser Way

An additional berm is also proposed along Harris Road, which will be almost 9 ft high from the sidewalk side.

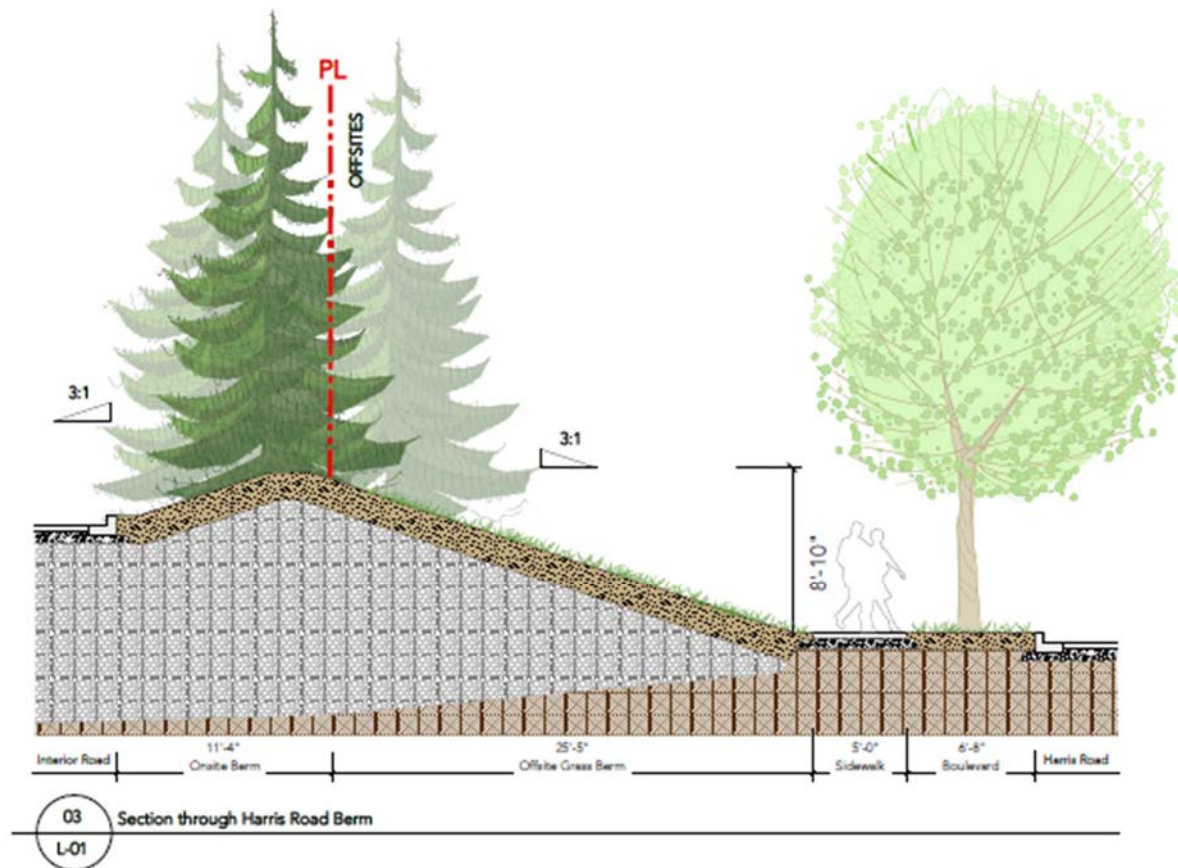


Figure 12: Section of berm along Harris Road

Post development, a new 4 m wide multi-use path will be available along the east and north property lines, with connections to Onni's Natures Walk townhouse development and the adjacent Sutton Ave neighbourhood. New sidewalks will be installed on Fraser Way and Harris Road, linking to trails along the Fraser River and connecting north towards the city centre.

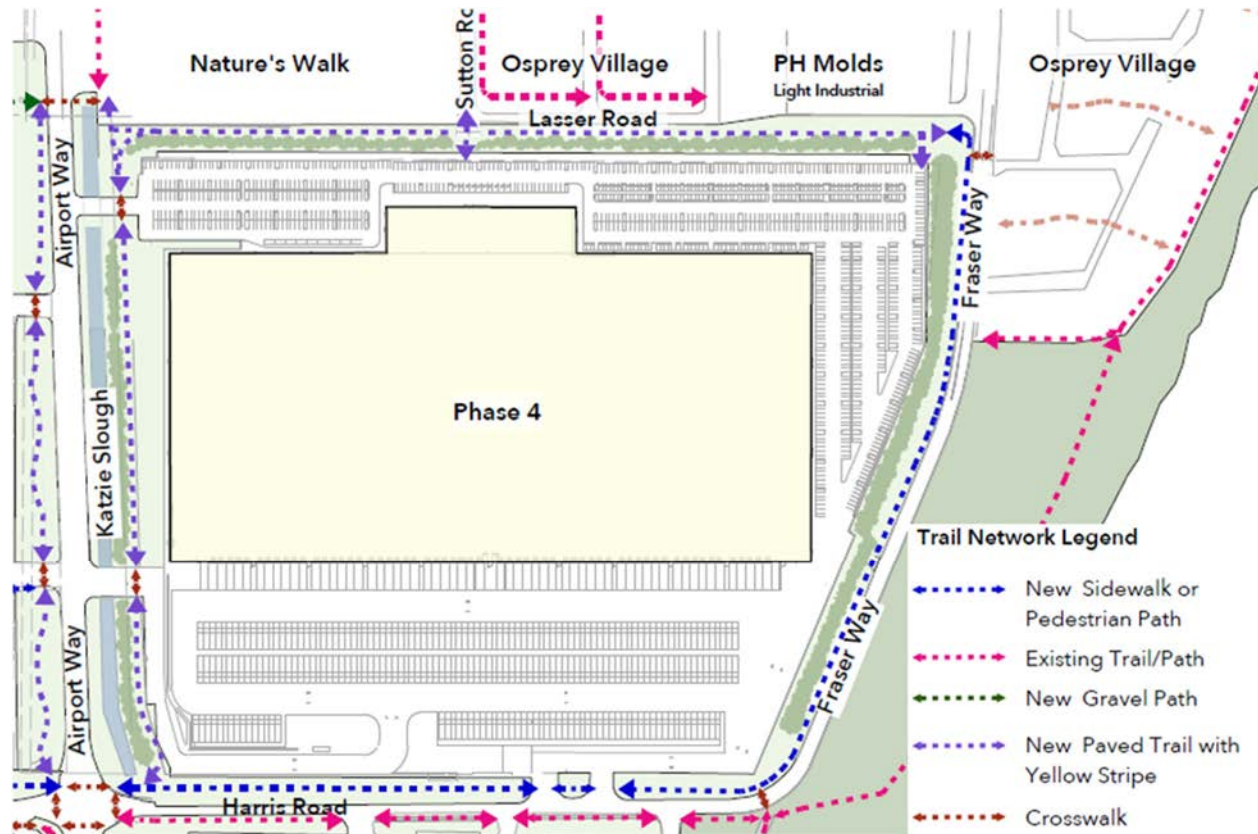


Figure 13: Pedestrian connections

As per the development covenant registered on-site, construction of the berms occurs concurrently with building construction. Berm construction is scheduled to begin during the provincially regulated instream works window of August 1 to September 15, 2021. However, this is contingent on some filling works being completed first and approvals being granted by the diking authority, the Province and the City for the berm along Fraser Way.

## Landscaping Security

The project's landscape architect has provided an overall estimate for the landscaping works, including hard and soft landscaping and site furnishings, for \$1,129,735.92. This amount, plus 10%, is required to be bonded as a development permit condition.

## Signage

A signage plan is included with the architectural drawings. A monument sign is proposed on the northeast corner of the Airport Way and Harris Rd intersection, as suggested by the DPA guidelines.

Some Sign Bylaw variances are requested to increase the number, height and area of free-standing directional and tenant signs permitted. Directional signs are the larger, free-standing signs proposed at the driveway entrances to direct traffic. Separate, also free-standing signs with the tenant name are proposed at the building corners.

The variances can be supported due to the large size of the site and the multiple access points. Emergency Services require signage such as proposed to easily and quickly identify where to go in case of an emergency. The regulations in the Sign Bylaw were designed for smaller sites with one access.

### **Sustainability Features**

Environmentally friendly design is a vital objective of the DPA guidelines. This development proposes the following:

- Solar shading to reduce solar heat gain through canopies and architectural fins, along with extensive tree planting throughout the site;
- "Solar ready" building;
- Conduit will be provided to a minimum of 10% of the employee and visitor parking stalls with tenant(s) to be responsible for adding charging stations as required;
- Charging station for electric shunt trucks to move trailers;
- Dual flush electronic sensor toilets; and
- "Future-proofing" of buildings can be adapted for re-use in the future, e.g. Knockout panels removed to add more interior units.

### **Uses**

At the Engagement and Priorities Committee meeting, there was discussion regarding potential uses of the site. While the I-3 zoning does permit some additional uses on the property, such as daycare, personal service, restaurant, micro-brewery etc., there is no requirement for these uses to be included. The main intent of the I-3 zoning is to permit light industrial uses, with light industrial being defined in the City's Zoning Bylaw as "a general industrial use that wholly enclosed within a building except for the outside storage of commercial vehicles, and that is not offensive by reason of smoke, noise, vibration, dirt, glare, or electrical interference." Typical light industrial uses include warehouse, distribution, manufacturing, packaging, processing, servicing and fabrication, occurring indoors.

At this development permit stage, there is no actual light industrial use occurring yet. Therefore it is impossible to predict whether any future tenant(s) might be in any sort of

contravention of the Zoning Bylaw. When a business is looking to locate in the building, a business licence is required, and the City reviews applications before approval of the business licence. Staff cannot confirm if the use(s) proposed by a potential tenant is permitted until a business licence application is received and reviewed. However, based on the development permit drawings submitted with this application, it appears that the site will be used as a warehouse and distribution centre, which is permitted in the I-3 zoning. Further, none of the activities that are typical of a warehouse and distribution centre are anticipated to be offensive by reason of smoke, noise, vibration, dirt, glare, or electrical interference, in conformance with the regulations in the I-3 zoning. If noise is found to contravene the City's Noise Bylaw, the legal agreement mentioned previously requires a follow-up acoustic study and lays out further mitigation options if deemed necessary by the study.

### **Conclusion**

Staff conclude that this project achieves the highest possible design level while acknowledging that a light industrial development has specific tenant needs. The City's Advisory Design Panel, members of the public and Council, have thoroughly reviewed this application and provided excellent technical advice and thoughtful design comments, many of which the developer could incorporate into the plans. Although quorum was not achieved at the final ADP meeting, members in attendance did support the application and noted that a motion of support would have been suggested, had quorum been achieved.

An analysis of the application against the Development Permit Area guidelines identifying how the proposal meets the intent of the guidelines was performed (Attachment A). In general, the development proposes a high standard of design for light industrial development that is superior to the Golden Ears Business Park Phases 1 and 2. It will improve pedestrian connectivity in the area. Overall, staff are satisfied that the project has been through an extensive review process, resulting in improvements to the original design proposal and can now recommend that Council consider approval of the development permit.

If Council declines approval of the development permit, then reasons should be provided to the developer as to why and how the proposal can be amended further so as to gain approval.

### **COUNCIL STRATEGIC PLAN ALIGNMENT**

- ☒ Principled Governance    ☒ Balanced Economic Prosperity    ☐ Corporate Excellence
- ☐ Community Spirit & Wellbeing                      ☐ Transportation & Infrastructure Initiatives
- ☐ Not Applicable

**Community Voice.** Engage stakeholders in meaningful discussion around the current and future success and prosperity of Pitt Meadows.



**Employment.** Help residents improve their quality of life by encouraging and sustaining diverse, well-paying employment opportunities close to home.

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### **FINANCIAL IMPLICATIONS**

☒ None      ☐ Budget Previously Approved      ☐ Referral to Business Planning  
☐ Other

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### **PUBLIC PARTICIPATION**

☒ Inform      ☒ Consult      ☐ Involve      ☐ Collaborate      ☐ Empower

Comment(s):

Members of the public provided feedback on and input to the form and character of design for Golden Ears Business Park Phase 4 through an EPC meeting.

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### **KATZIE FIRST NATION CONSIDERATIONS**

Referral      ☐ Yes      ☒ No

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### **SIGN-OFFS**

**Written by:**

Allison Dominelli,  
Development Services Technician

**Reviewed by:**

Alex Wallace,  
Manager of Community Development

Samantha Maki,  
Director of Engineering and Operations

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### **ATTACHMENT(S):**

- A. Checklist for compliance with DPA guidelines
- B. Development Permit No. 2021-005

## Development Permit Area No. 4 – Business Park

## Application checklist for compliance with design guidelines

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
6.4.4.1	<i>Form and Character</i>					
a)	Front facades shall be designed to be visually attractive by: i. Modulating the façade – stepping back or extending forward a portion of the façade; ii. Changing materials, colours, patterns and textures within the building plane; iii. Changing the rooflines to reinforce the façade modulation; and iv. Designing buildings with visually distinct bases, middle and upper storeys.				✓	
b)	The front façade theme should be extended around the building walls that are visible from all streets, public park spaces, and residential area.				✓	
c)	Main entrances to the buildings should be easily identified from streets or entry driveways.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
d)	Offices reception and other public areas should be located at the front of buildings, prominent and/or secondary frontages, and open spaces/parks.		✓			Main office located facing residential, away from prominent frontage but preferred to soften transition from residential to warehouse
e)	Shared service areas among buildings are encouraged (e.g. shipping and receiving, parking and outdoor lunch areas).	✓				Only one building
f)	Rooftop equipment (satellite dishes, air conditioning, etc.) are required to be screened from public view or integrated within the building architecture.				✓	
g)	If a request is granted by Council for the building height to exceed 12 metres (39 ft), the portion of the roof area exceeding 12 metres (39 ft) shall incorporate a green roof technology or energy-saving technology.	✓				
<b>6.4.4.2 Parking and Access</b>						
a)	Where possible, parking should be provided at the rear of the buildings.			✓		Parking located all over site. Only one building.

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
b)	Loading areas should be located at the side or rear of the buildings and should be screened from view. Where a loading area is facing the side yard, the overhead doors should be integrated into the overall design of the building.				✓	Fence and hedge screening around loading
c)	Loading areas should not be located adjacent to residential and park areas.				✓	Loading located as far from residential as possible
d)	Parking areas and vehicle circulation need to be designed to avoid conflicts between trucks, heavy vehicles and passenger vehicles.				✓	
e)	Pedestrian and cycling routes within industrial development need to be connected with routes in the surrounding areas.				✓	
f)	Pedestrian connections shall be provided that connect the north and south residential areas adjacent to industrial lands, as shown in the concept land use plan provided hereto in 1 (Design Guidelines prepared by IBI Group).				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
g)	Pedestrian connections shall be located in a buffer area that separates and screens industrial uses from adjacent uses. Buffers may range in size depending on the adjacent use. In no case shall the buffer be less than 15 metres (50 ft) where the buffer is immediately adjacent to a residential property.				✓	
6.4.4.3 <i>Landscaping and Screening</i>						
a)	Landscaped islands shall be used in parking areas to break-up large paved surfaces.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
b)	<p>Trees and shrubs should be planted throughout the parking area to intercept precipitation, reduce surface heating, and enhance aesthetics of the area. Trees should be planted within parking areas at the following frequency:</p> <ul style="list-style-type: none"> <li>i. One tree per six parking stalls to be located in a minimum of 10 cubic metres of growing medium to all maturity;</li> <li>ii. Two adjoining rows of parking stall (front to front) will require a minimum of one tree per bank of 12 parking stalls spaced no more than 18 metres between trees; and</li> <li>iii. The end of each single row of parking stalls will require a tree and a landscaped island between the end of the parking stall and the adjacent drive aisle.</li> </ul>				✓	
c)	Parking spaces should be designed in a way that a portion of the vehicle hangs over into a landscaped strip where possible.				✓	
d)	Landscaping should be used to identify entrances to the site and building.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
e)	Outside storage and staging areas should be screened with landscaping or screening finished in a manner consistent with the principal building.				✓	
f)	Ancillary or secondary buildings should be screened on a site from public streets with dense plantings.	✓				
6.4.4.4 <i>Green Technology Principles</i>						
a)	Buildings should be oriented to maximize opportunities for passive solar heating and natural lighting.				✓	
b)	Dual-flush toilets, electronic sensors on all lavatory faucets and low-flow shower heads should be considered in order to reduce the use of potable water.				✓	Dual flush electronic sensor toilets will be installed as part of the base building BP permits
c)	Water conservation and energy efficient design shall be incorporated into building and site design.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
d)	The use of permeable materials for parking areas, roads and sidewalks is recommended.			✓		Permeability of fill soil is not conducive to storm water infiltration. As a result, water is directed to detention tanks. Landscaping islands planned every six stalls and landscape strips at the head of parking stalls also improve storm water retention and permeability as do the perimeter berms/trails
e)	Opportunities to re-circulate water, including collected stormwater and high quality wastewater should be investigated and incorporated into a development project where possible.		✓			not needed for irrigation as drought tolerant plants used
f)	Parking and other paved areas should be designed to minimize the negative impacts on surface runoff volume and quality by installing oil/water separators for high traffic areas and directing runoffs to bio-swales and bio-filtration strips.				✓	SWMP – Includes Detention tanks to ensure post development run-off is equivalent or better than pre-development scenario. Oil-Water separator(s) will be provided in the parking areas. Landscape areas in the parkade provide additional infiltration.



OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
g)	A sustainability checklist shall be submitted with applications that demonstrate sustainability measures being incorporated to reduce environmental impacts. These should include measures such as green roof technology, energy and water conservation, and Green House Gas emission reductions.				✓	
6.4.4.5 LEED Certification						
a)	New Developments are encouraged to apply for LEED (Leadership in Energy and Environmental Design green building rating system) certification to assess the environmental sustainability building design.	✓				
6.4.4.6 Environmental Controls						
a)	Industrial developments should be planned so that the uses and activities do not create nuisance such as the emission of odours, dust, fumes, noise or light pollution.				✓	
b)	Garbage and waste materials should be stored in waterproof and animal resistant containers that are visibly screened from the public and employees.	✓ to be provided by tenants				
6.4.4.7 Crime Prevention through Environmental Design (CPTED)						

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
a)	The application of CPTED principles for the building design, lighting, landscaping and other elements should be applied.				✓	
6.4.4.8 <i>Stand-Alone Commercial Uses</i>						
a)	Development Permit Area No. 8 South Loughheed Commercial guidelines shall apply to stand-alone commercial uses in an industrial park.	✓				
6.4.4.9 <i>Site Safety Design</i>						
a)	A safety audit checklist shall be submitted with applications that demonstrate safety elements incorporated into the design of the trail networks, and the industrial business park.				✓	
b)	The following factors should be taken into account when designing the exterior environments in the industrial business park: <ul style="list-style-type: none"> <li>• Visibility by others (design for seeing and being seen);</li> <li>• No entrapment spots (avoid small areas shielded on three sides); and</li> <li>• Lighting (others' faces should be visible and blinding glare avoided).</li> </ul>				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
c)	Site access and internal circulation should be designed to emphasize public safety at the intersections of public and private domains, internal security and efficient flows. Safe movement of pedestrians should be a priority above other modes of transportation.				✓	
<b>Schedule – South Harris Business Park Design Guidelines (prepared by IBI Group)</b>						
<b>Streetscape</b>						
	To create a lush landscaped edge around the business park, streetscape improvements should be provided within a minimum five metre setback along public streets. Within the setback a rural or natural landscape approach may be used, incorporating trees that provide a full canopy with a well-articulated supporting understory.				✓	
	Along Harris Road and Airport way, more formal urban streetscape elements and planting patterns may be incorporated on corners and at entry points.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Careful attention should be given to the interface between the business park and Katzie Slough, which runs along Airport Way, to ensure the landscape and drainage plans for the site contribute to the enhancement of this sensitive riparian area. A landscaped buffer with appropriate plantings to intercept and filter surface run off should be a defining feature of the Airport Way frontage on the south side.				✓	
	Pedestrian and cyclist connections into the business park should be compatible with the external streetscape and provide a seamless transition to the internal circulation network. Well-defined walkways should link with sidewalks to ensure pedestrians can safely travel from sidewalks to internal destinations, and where entrances are located on a bike route, adequate space should be provided to allow for vehicles and cyclists to enter and exit the business park at the same time.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Where the greenway meets adjacent streets (refer to the Comprehensive Plan on page 5), a transitional area should be provided between the end of the greenway and the streetscape. Bollards (formal or natural, e.g. boulders or wooden fencing) should mark the transition between the greenway and the upcoming street to encourage cyclists to slow down or dismount. The transition from the greenway to the street should be fully accessible, and integrated with sidewalks and the bike lane along Airport Way.				✓	Bollards between Greenway and MUP to encourage cyclists to slow down.
	Streets and walkways should be appropriately lit to ensure the safety of vehicles, cyclists and pedestrians and landscape elements should comply with CPTED provisions for natural surveillance.				✓	
Greenway						
	The greenway should provide a landscape buffer between business park uses and adjoining residential neighbourhoods and establish an off-street pathway link to/from the Central Business District, Fraser River, Athletic Park, and the South Harris/Bonson community.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	The greenway corridor should incorporate: a three metre wide, multiuse pathway accessible by cyclists and pedestrians, consistent with the standards of the Pedestrian and Cycling Master Plan; mature landscaping; wayfinding signage; site furniture; pedestrian scale lighting designed to allow safe use at night; and special attention to safety considerations where the greenway meets adjacent streets.				✓	Landscape plans show 4 M multi-use path
	Where possible, consider integrating storm water management features such as bioswales and permeable surfaces into the greenway corridor.	Council requested a paved path, not a permeable surface.			✓	Ample landscaping for storm water infiltration i.e. landscape strips at the front edge of parking stalls, landscape islands every six parking stalls
	The width and treatment of the greenway corridor may vary but must remain a continuous public pathway and landscaped buffer incorporating the elements below.				✓	Greenway trail to be consistently 4m

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	The greenway corridor should be accessible from all sides in order to encourage use by both the public and business park users. Where conditions such as swales or grade changes exist, footbridges or steps should be provided where logical connections between the internal network of the business park and community emerge.				✓	
	Unless necessary for safety, avoid the use of fences to reduce the hard edge interface between the business park and the greenway corridor.				✓	Fencing only provided around loading area for security
	The greenway planting design should carefully balance the need for passive surveillance and screening with creation of a meaningful separation. Consider the use of lower level native planting as an understory to deciduous and evergreen trees that can be standardized in accordance with CPTED principles to provide natural surveillance.				✓	
	Seating opportunities should be located to provide resting places along the greenway at key points of interest, while minimizing opportunities for unobserved loitering.				✓	
Buildings						

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	A coordinating architect shall be engaged to administer and monitor implementation of the design guidelines.				✓	
	Strong architectural expression is required along Harris Road and Airport Way frontages, as indicated on the Comprehensive Plan. Signature buildings and/or significant architectural features should be focused at the intersection of Harris Road and Airport Way, and at main entrances to the business park. On corners, architectural details should wrap around to address abutting streets and all sides of a building should be visually appealing.				✓	
	Architectural expression may vary between buildings, but should incorporate complimentary palette of colours, materials, composition and massing.				✓	
	Buildings should be oriented to address public streets and main internal circulation streets, with primary public functions such as offices, showrooms, restaurants or other on-site services located at the front of buildings.			✓		Office/front located at east side of building, to provide transition to residential area. Only one building.



OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Office and public entrances should be highlighted with higher quality finishes, lighting, weather protection, and significant glazing. Ground floor glazing should be provided for a minimum of 50% of the façade length where facing a public street.			✓		50% achieved along Airport Way; 48% achieved along Fraser Way; 8.2% achieved along Harris Rd (loading bays)
	Loading bays should be architecturally expressed through simple recessing and reveals, and with glazing, lighting, or focal elements to enhance visual appearance.				✓	
	Along secondary frontages and where not facing a public street or punctuated by loading bays, walls should be of a high quality material that provides a pattern as part of the finish, or changes in colour, secondary material, or glazing; or a focal element should be added to provide a change in composition. Other strategies may be used to modulate the massing of the façades at the discretion of the approving authority.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	<p>In order to modulate the visual impact of long façades, a combination of the following strategies should be employed:</p> <ul style="list-style-type: none"> <li>- Create articulated steps in plan of approximately 60cm or more;</li> <li>- Vary heights at the roof line;</li> <li>- Add focal elements by way of changes in form, materials or colour.</li> </ul>				✓	
	<p>In order to modulate the visual impact of building heights, a combination of the following strategies should be employed:</p> <ul style="list-style-type: none"> <li>- Create articulated bays extended or recessed approximately 60cm or more from the façade;</li> <li>- Incorporate cornice and/or base features;</li> <li>- Introduce changes in materiality along horizontal planes, such as clerestory glazing; and</li> <li>- Set back or cantilever out upper portions of the building, where internal function permits.</li> </ul>				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Buildings should use high quality, durable materials such as composite metal panel, architectural insulated metal panel, glass (vision or translucent), or precast concrete. Corrugated metal, wood, natural stone, brick or masonry elements, and other high quality materials may be used for design accents. Stucco and vinyl are not permitted. Other high quality materials may also be permitted at the discretion of the approving authority. Primary materials should be graffiti resistant.				✓	
	<p>Incorporation of sustainable building technologies to improve the environmental performance and employee comfort of buildings is encouraged and may include:</p> <ul style="list-style-type: none"> <li>- green roofs to enhance stormwater interception;</li> <li>- reflective white roofing to reduce the heat island effect;</li> <li>- smart sustainable building materials and systems, passive heating/cooling, opening windows, and natural daylighting to reduce energy consumption and improve employee comfort;</li> <li>- use of alternative energy sources;</li> <li>- water conservation and re-use.</li> </ul>				✓	<p>Considerable glazing provided for employee comfort. Solar shading provided for reducing solar heat gain, through the use of canopies and architectural fins. Future proofing for adding solar panels on the roofs of the Phase 4 building. Conduit provided and structurally designed for additional weight from solar panels.</p>

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Where sustainability elements such as green roofs, solar panels or similar technology are visible, they should be incorporated into the building design and not detract from the building's appearance.	✓				
Landscape						
	A coordinating landscape architect shall be engaged to oversee all landscape elements, including the greenway and streetscape design, site layout, parking, loading, pedestrian routes, lighting and signage.				✓	
	The landscape design should create an attractive, human-scaled environment for visitors and employees. The character of the landscape should combine a well-defined lush edge comprised of a healthy community of native plants punctuated with formal planting arrangements at each site entry. The on-site landscape should be more formal in character and provide ample shading of hard surfaces and buildings in the summer.				✓	Where landscaping doesn't shade buildings, solar shading devices included such as architectural fins and canopies

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	To establish the intended landscape character, trees at entries, in parking areas, and adjacent to buildings should meet minimum city street tree size requirements and be relatively mature at the time of planting. Wherever possible, existing mature vegetation should be retained.				✓	
	Landscape elements should reinforce the character of the site, which ranges along a continuum from "urban" at high traffic intersections to "rural" or "natural" along the site edges and rear yards. Urban areas should incorporate a more formal and geometric arrangement of soft and hard materials; rural treatments should use agricultural patterns of hedge rows and wind breaks; while natural areas should use more organically shaped arrangements and materials referencing the Fraser River, Katzie Slough and regional environments.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Site and building entries should be reinforced with signage and formal landscape elements. This could include incorporating cultural landscape tree planting patterns such as allees or groves of trees to create a tie to the agricultural history of Pitt Meadows, contribute to a meaningful sense of place, and provide a comfortable human environment.				✓	
	East-west pedestrian routes should be established to allow for comfortable internal circulation and provide connections between the business park and community at logical points (e.g. at Sutton Ave and the Athletic Park). Trees and landscaped boulevards should be used to define these routes, separate walkways from vehicles, and provide shade.				✓	Numerous walkways provided in the parking areas for pedestrian safety. Connections between Greenway and residential to the East. Additionally, ped crosswalk added at the Southern terminus of the greenway to connect the Greenway to the residents to the South.
	Where building facades are adjacent to parking areas, landscape elements should be incorporated into the site design to break up the building mass, soften the transition, and provide a human scale condition.				✓	Trees located every six parking stalls. East elevations include green wall elements.

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Landscape design should enhance the natural landscape and promote the use of native, drought tolerant and low maintenance plant materials. All planting and plant material conform to the latest edition of BCSLA (BC Society of Landscape Architects) and BCNLA (BC Nursery and Landscape Association) Standards.				✓	
	To ensure the long-term health and viability of trees, a minimum of 15 cubic meters of growing medium should be provided for each tree. Additionally, all landscape materials should be irrigated. Once plants are fully established, irrigation systems should be automated to respond to conditions and conserve water.				✓	Structural Soil in the parking lot included on Landscape DP drawings as previously designed.
	Landscape treatments and plantings should encourage water harvesting and include water conserving practices and techniques.				✓	Providing for infiltration in large landscape areas.
	A complimentary family of site furnishings and appointments such as benches, tables, bicycle racks, refuse/recycling/organic containers, paving material, lighting, plant materials, and signage should be selected to reinforce a unifying site character.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Site furnishings and appointments should be selected for durability, maintainability and appearance. They should incorporate recycled and 'green' material standards, while maintaining a high visual quality.				✓	
	Landscape elements should support CPTED principles by maintaining sight lines and allowing for natural surveillance, particularly for parking areas, the greenway, and amenity spaces.				✓	
Employee & Community Amenity						
	Indoor and outdoor amenity spaces should be provided within the business park and should be designed and located to enhance the experience of employees and visitors to the site and individual buildings.				✓	Indoor added via TI's. Significant outdoor amenities designed, including expansive trail network on the N side of the proposed building



OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	The amount of outdoor amenity space provided should accommodate the number of employees generated on-site as well as potential use by the public. As a general metric, 20-30% of employees should be able to utilize outdoor spaces at any one time, and each user should be provided with 20-30 square feet of space. The distribution of amenity space may vary across the site but an amenity space should be located within a five minute walk of each building.				✓	Ample Amenity spaces provided – MUP, Greenways and seating nodes on site. Park within 5 minute walk
	Where adjacent to features such as the Trans-Canada trail along the Fraser River, Pitt Meadows Athletic Park, bike routes and trail connections, amenity spaces should be designed to enhance visual and physical access to and from these locations for business park users and the general public.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Amenity spaces intended primarily for employees should be located throughout the business park in locations that are easy to access quickly: directly outside main entrances and office spaces; in close proximity to secondary/staff entrances; or on upper floors where balconies or rooftop patios can take advantage of views and sunlight exposure.				✓	Picnic tables, benches, seating areas distributed across site, including directly outside main entrance.
	<p>The design of outdoor amenity spaces should seek to:</p> <ul style="list-style-type: none"> <li>- achieve a diversity of shaded and sunny spaces through all seasons;</li> <li>- provide a variety of seating arrangements for groups and individuals;</li> <li>- incorporate high quality landscape materials and site furnishings;</li> <li>- offer weather protection; and</li> <li>- accommodate a range of activities, from passive to active.</li> </ul>				✓	
	As suggested in the OCP, a significant amenity such as a park or sports field may be located in the northeast corner adjacent to the Athletic Park, as generally indicated on the Comprehensive Plan.				✓	Phase 3
Lighting						

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	A comprehensive lighting plan with a unified family of lighting fixtures should be established for streets, buildings, pedestrian walkways, building entries and parking areas.				✓	Details on BP Drawings
	As much as possible, lighting should be designed to provide safe and functional levels of light in the business park, while minimizing sky glow, light trespass, and glare that may impact surrounding residents.				✓	Refer to Electrical BP Drawings
	Lighting fixtures, including those in parking and loading areas, should be dark-sky certified or equivalent to shield the light source, minimize glare and light trespass, and provide better vision at night.				✓	
	Entrances to buildings should be illuminated to enhance their prominence.				✓	
	Where mounted lighting is proposed on the exterior of a building, it should be integrated with the architectural design of the building.				✓	
	Any exterior building lighting should be oriented downwards to illuminate intended areas and limit glare.				✓	
	In parking areas, freestanding light poles should be located within landscaped islands.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Along the greenway, bollard lighting should be provided to enhance visibility and safety at night.				✓	Pedestrian scale lamp standards used for CPTED vs. Bollards
	Low height and bollard lighting in public areas should be tamper-proof to prevent vandalism, particularly along the greenway.				✓	
	LED lighting should be used wherever possible.				✓	
Signage						
	A comprehensive signage plan with a unified design that reflects the building and landscape character of the site and sets out a consistent hierarchy should be established for all site, building, and wayfinding signage.				✓	
	A significant gateway feature/monument sign at the corner of Harris Road and Airport Way should incorporate elements consistent with the building and landscape character of the business park.				✓	
	Corporate signage should be of a high quality, and should be architecturally integrated with the building design.				✓	
	Signage adjacent to streets should be less than 3 metres in height.				✓	
Parking, Loading & Storage						
	Site design should seek to separate vehicle parking from loading areas.				✓	

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	All parking areas located along the outer edges of the business park should be well landscaped to provide visual screening in combination with streetscape improvements.				✓	Parking along the perimeter generally has a landscape buffer between the parking stalls and the adjacent lot.
	Parking areas should be broken into small sections, with landscape strips approximately every 6 stalls at least three metres in width, planted with shade trees, shrubs and ground covers.				✓	Landscape strips with trees located every six stalls
	Surface water should be directed to planted areas in parking lots to facilitate infiltration and reduce pollutants with appropriate plant materials in bioswales or raingardens.				✓	Generally surface water directed to detention tanks to ensure pre-development storm water retention matches post - development. However, landscape strips at the head of parking stalls are provided to allow for additional storm water retention.
	Best practices in stormwater management techniques should be incorporated, including: directing surface water into planted areas to facilitate natural infiltration and reduce pollutants through bioswales or raingardens; and using permeable surfaces to allow for groundwater recharge. Impervious surfaces should be limited to 65%, where possible.			✓		Generally surface water directed to detention tanks to ensure pre-development storm water retention matches post - development. Impervious surface is 86%.

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Concrete wheel stops are discouraged in favour of parking stalls designed to allow for vehicles to overhang landscaped areas or sidewalks by 0.6 metres. The required length of the stall may include the overhang.				✓	Landscape areas at the head of the parking stalls allowing vehicles to overhang.
	Pedestrian routes to building entrances or other key destinations through parking lots should be clearly marked and preferably separated from vehicle traffic with landscaping.				✓	Numerous pedestrian paths designed for safe passage in the parking lots. Signage via TI's will ensure pathways are clearly marked.
	Loading areas should be located in the rear of buildings and oriented to the interior of the site. Loading bays are not permitted along prominent or secondary frontages.			✓		No loading facing Airport Way, Fraser Way or the Residents to the East. Loading planned along the west elevation.
	Refuse, recycling, organic containers and utility kiosks should be located in rear yards and screened from view with a combination of architectural elements, landscape, and fencing. Containers should be secure and not attract wildlife, pests, or generate odour.	✓				Tenant Guidelines will enforce proper usage of waste recycling kiosks. Screening etc. will be determined via TI Drawings

OCP Section	Guideline	Not Applicable	Non-Compliant	Partial Compliance	Substantial Compliance	NOTES
	Fencing should be avoided unless required for specific screening, security or safety reasons. Any fencing should be transparent and of high quality (e.g. lattice or wrought iron).				✓	Fencing only located around loading area for security/screening reasons
	Doors and windows should provide natural surveillance of parking and loading areas and lighting at a variety of scales should illuminate areas that will be accessed at night.				✓	Substantial glazing provided. Dock and Man doors specified to include glazing – Refer to BP drawings

**CITY OF PITT MEADOWS**

**DEVELOPMENT PERMIT NO. 2021-005**

THIS DEVELOPMENT PERMIT made in triplicate and dated the      day of      ,  
2021

ISSUED BY:

**THE CITY OF PITT MEADOWS**, a City under the "Local Government Act" of the Province of British Columbia, and having its Municipal Offices at 12007 Harris Road, in the Municipality of Pitt Meadows, in the Province of British Columbia, V3Y 2B5  
(hereinafter called the "City")

TO:            **0801968 B.C. LTD.**  
                 200 - 1010 SEYMOUR ST  
                 VANCOUVER BC V6B 3M6

hereinafter called the "Permittee(s)")

WHEREAS the Permittee(s) wishes to commence a development upon ALL AND SINGULAR those certain parcels or tracts of land and premises situate, lying and being in the The Corporation of the City of Pitt Meadows in the Province of British Columbia, and more particularly known and described as:

**Parcel Identifier:**    011-338-385

**Legal Description:** Lot 1 District Lot 254 Group 1 New Westminster District Plan 8763

**Parcel Identifier:**    011-338-415

**Legal Description:** Lot 2 District Lot 254 Group 1 New Westminster District Plan 8763 Except Plan BCP49951

(hereinafter called the "Lands")

AND WHEREAS the Permittee(s) has made application for a Development Permit in regard to the Lands;

AND WHEREAS the Local Government Act provides that in such a Permit certain matters may be regulated, required or limited; and



NOW THEREFORE, the Council of the City hereby issues a Development Permit in respect of the Lands as follows:

1. This Development Permit is issued subject to compliance with all City Bylaws except where specifically varied or supplemented by this Development Permit.
2. The Permittee(s) shall comply with all City and Provincial building regulations and shall not commence work until they have received a Building Permit from the City.
3. The Lands shall be developed by the Permittee(s) in strict conformity with the terms and conditions set out in this Development Permit.
4. The Permittee(s) shall not sell, assign, transfer, convey or otherwise dispose of all or any part of his interest in and to the Lands, save and except for mortgage purposes, until the Lands have been developed in accordance with the conditions of this Development Permit, or alternatively, until the proposed purchaser or assignee has covenanted with the City, in a manner acceptable to the Council of the City, to ensure development of the Lands in conformity with the conditions herein contained. After the successor or assignee of the applicant has covenanted in writing to the satisfaction of the Council of the City to assume all of the Permittee's obligation and liabilities herein, the obligations and liabilities of the Permittee(s) shall cease in regard to development work done thereafter by the said successor assignee.
5. All plans, specifications, minutes and written commitments referred to above are subject to any changes required by the Building Official or their designate, or other officials of the City, where such plans and specifications do not comply with any duly enacted law or Bylaw, and such non-compliance is not specifically permitted by this Development Permit; minor variations which do not substantially alter the work referred to in Part 6 and 7 may be permitted if approved in writing by the Director Responsible for Development Services or their designate (hereinafter called the "Planner") thereon.

As a condition of the issuance of this Development Permit, the City is holding security in the form of an Irrevocable Letter of Credit as follows:

Securities totaling the amount of **\$1,248,358.19** for the on-site landscaping works are to be provided prior to Building Permit issuance to ensure that the development specified in Part 6 i) and 6 ii) hereto, and particularly the landscaping works therein, is carried out in accordance with the terms and conditions of this Development Permit, and to ensure that an unsafe condition will not result from a contravention of the provisions of this Development Permit.

The condition of the posting of the security is that should the Permittee fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security and any interest thereon accrued by the City to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Permittee(s), or should the Permittee(s) carry out the development permitted by this Development Permit under the conditions set out in Paragraph b) hereto, the security and any interest thereon accrued by the City shall be returned to the Permittee(s) upon:

- a) Issuance by the Building Official of an Occupancy Permit where applicable;
  - b) Verification by the Planner that the development set out in Part 6 i) and 6 ii) hereto, and particularly the landscaping works therein, have been completed in accordance with the terms and conditions in this Development Permit;
  - c) Written confirmation from a qualified professional as to the acceptable environmental quality of fill used for the berms, as well as an acceptable compaction level, to the satisfaction of the City; and
6. Development upon the Lands shall conform to the following plans, specifications, and the conditions:
- i) Set of plans and specifications prepared by TKA+D Architecture + Design Inc., #305 – 1930 Pandora St, Vancouver, BC, entitled “Golden Ears Industrial Park Phase 4 – Build to Suite – Building 4000”, copies of which are attached to and form part of this permit;
  - ii) Set of plans and specifications prepared by Durante Kreuk Ltd., #102 – 1637 West 5<sup>th</sup> Avenue, Vancouver, B.C., V6J 1N5 entitled “Golden Ears Industrial Park Phase 4”, copies of which are attached to and form part of this permit; and
  - iii) Development Covenant CA6856082 registered on title.
7. Garbage and recycling receptacles are to be contained within the units and wheeled out for collection.
8. Prior to any construction or land development works within 30 m of the top of bank of any watercourse or ditch, approval is required under the provincial *Riparian Areas Protection Regulation*.
9. Trail and path wayfinding signage shall be designed in accordance with City standards for pedestrian and park signage.
10. The developer shall be responsible to connect all trails and paths to existing City trails and paths to the City’s satisfaction, and include lighting on paths.

11. Rooftop HVAC units and any other rooftop mechanical equipment shall not be visible from any public street.
12. Class I bicycle parking spaces are to be distributed evenly throughout the development and provided by tenants.
13. At least 10% of the employee and customer parking stalls shall have infrastructure for electric vehicle charging capability.
14. Internal traffic calming measures may be added to the site on an as-needed basis identified by the developer.
15. Approval from the diking authority is required prior to berm construction along Fraser Way.
16. An amendment to this development permit is not required the building is split into separate units and additional office spaces added, provided any exterior changes are consistent with the surrounding exterior design and the plans attached to this permit.
17. Variances to Section 7.2 of the Sign Bylaw No. 2715, 2015 have been identified and are authorized by this permit to vary the number of, height and area of directional and tenant signs to ensure appropriate entry for the scale of the development, in accordance with the attached plans.
18. In the event the Permit lapses, the Permittee(s) may request refund of the security described in Part 5 and the City shall make refund provided the Director of Operations and Development Services or designee authorizes such refund.
19. It is understood and fully agreed by the Permittee(s) that the site and abutting streets shall be maintained in a neat and tidy order during the construction process, and that construction debris bins will be removed from the site promptly once they are full.
20. Whenever the singular or masculine is used in this Development Permit, the same shall be deemed to include the plural, or the feminine, or the body politic or corporate as the context so requires, and every reference to each part hereto shall be deemed to include the heirs, executors, administrators, successors and assigns of such party whenever this context or the parties so require.
21. It is understood and agreed that the City has made no representations, covenants, warranties, guarantees, promises or agreement (verbal or otherwise) with the Permittee(s) other than those in this Development Permit.
22. If the Permittee(s) does not substantially start any construction with respect to which this Permit was issued, within 2 years after the date it is issued the Permit shall lapse.

23. The terms of this Development Permit or any amendment to it, are binding on all persons who acquire an interest in the Land.

AUTHORIZED BY COUNCIL RESOLUTION passed by on the      day of

**THE CORPORATE SEAL OF THE CITY OF PITT MEADOWS** was  
hereunto affixed on the      day of      , 2021.

\_\_\_\_\_  
Bill Dingwall, Mayor

\_\_\_\_\_  
Kate Barchard, Corporate Officer

\_\_\_\_\_  
*Owner of the Lands or authorized signatory(ies)*

\_\_\_\_\_  
*Print Name*

SIGNED, SEALED AND DELIVERED BY  
the Owner of the Lands on  
the \_\_\_\_\_ day of \_\_\_\_\_, 2021

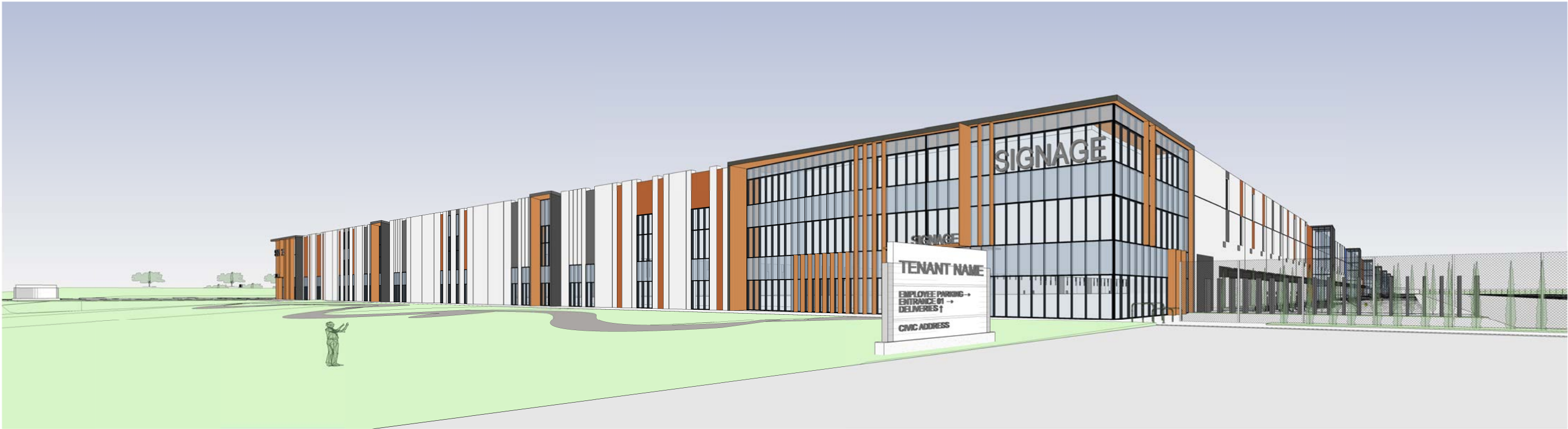
### PDF Files of Development Permit Drawings:

Architectural plans prepared by TKA+D Architecture + Design Inc., #305 – 1930 Pandora Street, Vancouver, BC, entitled “Golden Ears Industrial Park Phase 4 – Build to Suit – Building 4000” and dated May 3, 2021:

A000	-	Cover Sheet	Page 7
A001	-	Project Data	Page 8
A003	-	Context Plan	Page 9
A004	-	Design Rationale	Page 10
A005	-	Precedents Images	Page 11
A006	-	3D Views	Page 12
A007	-	Rendering Image	Page 13
A010	-	Signage Location	Page 14
A011	-	Site Signage	Page 15
A012	-	Building Signage	Page 16
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A102	-	Site Plan - NE	Page 18
A103	-	Site Plan - SE	Page 19
A104	-	Site Plan - W	Page 20
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A400	-	Building Section	Page 29
A401	-	Building Section	Page 30

Landscape plans prepared by Durante Kreuk Ltd., #102 – 1637 West 5<sup>th</sup> Avenue, Vancouver, B.C., V6J 1N5 entitled “Golden Ears Industrial Park Phase 4 Pitt Meadows” and dated April 29, 2021:

L-0	-	Key Plan	Page 31
L-1	-	Context Plan	Page 32
L-2	-	Landscape Site Plan	Page 33
L-3	-	Landscape Site Plan	Page 34
L-4	-	Landscape Site Plan	Page 35
L-5	-	Landscape Site Plan	Page 36
L-6	-	Landscape Site Plan	Page 37
L-7	-	Landscape Site Plan	Page 38
L-8	-	Landscape Site Plan	Page 39
L-10	-	Landscape Site Plan	Page 40
L-11	-	Landscape Site Plan	Page 41
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L-13	-	Sections	Page 43
L-14	-	Sections	Page 44
L-15	-	Details	Page 45



Golden Ears Industrial Park  
Phase 4 - Build to Suit - Building 4000

Re-Issued for Development Permit - 2021 May 3rd

TKA+D  
ARCHITECTURE + DESIGN INC.  
200 - 100 HARRIS ROAD, PITT MEADOWS, BC V3Y 6P1  
GEBP - PH4 - Build to  
Suit  
11208 Harris Road,  
Pitt Meadows  
Cover Sheet  
PLOT DATE: 2021-05-03 8:37:55 AM  
A000

# GEBP - PH4 - Build to Suit

## Onni Group

### 11208 Harris Road, Pitt Meadows

## Re Issued for DP submission

### ZONING SUMMARY

PROJECT NAME	Golden Ears Business Park Phase 4 - Built to Suit Option
PROJECT ADDRESS	19300 Airport Way 11208 Harris Road
AUTHORITY	City of Pitt Meadows
ZONE	I-3
LEGAL DESCRIPTION	(Phase 4): LOT 1 - DL 254
USES	Light Industrial
SITE AREA	(47.69 acres)
PH4 - A Gross Area	2,073,613 SF 192,645 m²
PH4 - B Developable Area 1 (Setback)	1,934,307 SF 179,703 m²
PH4 - C Developable Area 2 (SPEA)	1,914,402 SF 177,854 m²
Refer to the Keyplan 3 for outline sketches of Areas A, B and C.	
*Area A (Gross) used for Density and Lot Coverage Calculations	

DENSITY (FAR )	1.0 0.42**
Permitted:	Not Applicable
Proposed (Phase 4):	41.7%***
**Total Gross Floor Area / Site Area (Gross)	
LOT COVERAGE	
Permitted:	Not Applicable
Proposed (Phase 4):	41.7%***
***L1 Gross Floor Area / Site Area (Gross) x 100	
HEIGHT	
Permitted:	12.0 m (39.37 ft)
Main height Proposed :	11.9 m (39 ft)
Pop out and front entrances	40ft
SETBACKS	
Front Yard : Airport Way (North):	
Required:	7.5 m (24.6 ft)
Proposed:	(min.) 41.8 m (137ft)
Side Yard (East):	
Required:	7.5 m (24.6 ft)
Proposed:	(min.) 50 m (164 ft)
Rear Yard (South):	
Required:	7.5 m (24.6 ft)
Proposed:	(min.) 21.1m (69'-2")
Side Yard (West):	
Required:	7.5m (24.6 ft)
Proposed:	(min.) 157.9 m (508'-3" ft)

### AREA SUMMARY

Area Summary - By Level		
Description	Area (SF)	Area (SM)
Level 1	863,785 SF	80,248 m²
	863,785 SF	80,248 m²
Area Summary - By Occupancy		
Occupancy	Area (SF)	Area (SM)
Industrial	818,822 SF	76,071 m²
Office	44,963 SF	4,177 m²
	863,785 SF	80,248 m²

### PARKING REQUIREMENTS

Parking Dimensions	
Regular Stall	5.5 m (18 ft) x 2.6 m (8.53ft)
Small Stall ( MAX. 25%)	4.8 m (15.75 ft) x 2.4 m (7.87 ft)
Stall for persons with disabilities	5.5 m (18 ft) x 3.6 m (11.8 ft)
Parallel Stall	6.7 m (22 ft) x 2.6 m (8.53ft)
Aisle Dimensions	
Aisle (2 Way Traffic)	7.0 m (23ft)
Aisle (1 Way Traffic)	3.8 m (12'-6")
Parking Ratios	
Industrial	1/100 m² (1/1076 ft²)
Office	1/45 m² (1/484 ft²)
Parking Required for Persons with Disabilities	
- As per BCBC 2018 [3.8.3.4.(2)]	1/100 parking stalls

### PARKING PROVIDED

Note : Additional parking is provided beyond bylaw requirements to meet specific needs of the tenants and their intended uses.			
Parking Required - Phase 4			
Occupancy	Area (SF)	Parking Req'd	Spaces
Industrial	818,822 SF	1,076.0	761
Office	44,963 SF	484.0	93
	863,785 SF		854
Required:	1 per every 100 stalls provided for all other uses		
Provided:	9		
Required Parking Space for Persons with Disabilities			

### Total Parking Provided

STALL TYPE	COUNT
	9
DISABILITY 3.6m x 5.5m	10
REGULAR 2.6m x 5.5m	728
SMALL CAR 2.4m X 4.8m	168
23% of small car provided	
	915

### LOADING REQUIREMENTS

Note : Not all truck movements to all dock positions may be possible with trucks and/or trailers parked in all bays	
Large Bay Dimensions:	3 m (9.9 ft) x 9 m (29.5 ft)
Required:	NA

### Loading provided

STALL TYPE	COUNT
DOCK LOADING BAY	69
GRADE LOADING BAY	2
TRAILER PARKING	248
	319

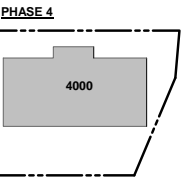
### BICYCLE PARKING

Required for Industrial Use	
Required Class 1	10% of the Parking requirement
Required Class 2	Not required
Dimensions (Class 1)	1.8 m ( 5.9 ft) x 0.6m (2 ft)
Required for PH4 :	77 stalls
Provided	Bike Enclosure Provided per tenants
Required for Office Use	
Required Class 1	1 per 750m² gfa
Required Class 2	Min 6 spaces for developments greater than or equal to 2,000 m²
Dimensions (Class 2)	Min Aisle clearance : 1.2m
Req'	Class 1 : 6 Class 2 : 6
Provided	Bike Enclosure Provided per tenants

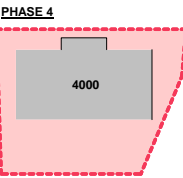
Total Required	
Required Class 1	83 stalls
Required Class 2	6 stalls



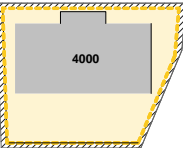
### KEYPLAN



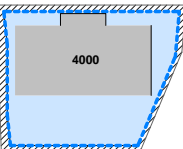
### KEYPLAN 3



AREA A (RED) : Existing PL



AREA B (YELLOW): 7.5m SETBACK from existing PL



AREA C (BLUE) : 7.5m SETBACK FROM EXISTING PL + Katie Slough.

## CONTEXT PLAN



### GRAPHIC & SYMBOL LEGEND :

Existing construction; to be retained		Detail Reference
Existing construction; to be demolished		Wall / Detail Section Reference
New stud wall; construction as indicated		Building Section Reference
New insulated stud wall; construction as indicated		Interior Elevation Reference
Demolition Tag		Matchline View Reference
Room Tag		Grid Reference
Door / Window Tag		Elevation / Level Reference
Wall / Floor / Roof Type Tag		Spot Elevation (Relative to Topographical Survey)
Material / Finish Keynote Tag		Spot Elevation (Relative to Project Base)
Millwork Tag		North Arrow
Specialty Equipment Tag		Ceiling Type Tag
Centre Line		
Property Line		
Fire Hydrant		
Manhole Cover		
Catch Basin		

### DRAWING INDEX:

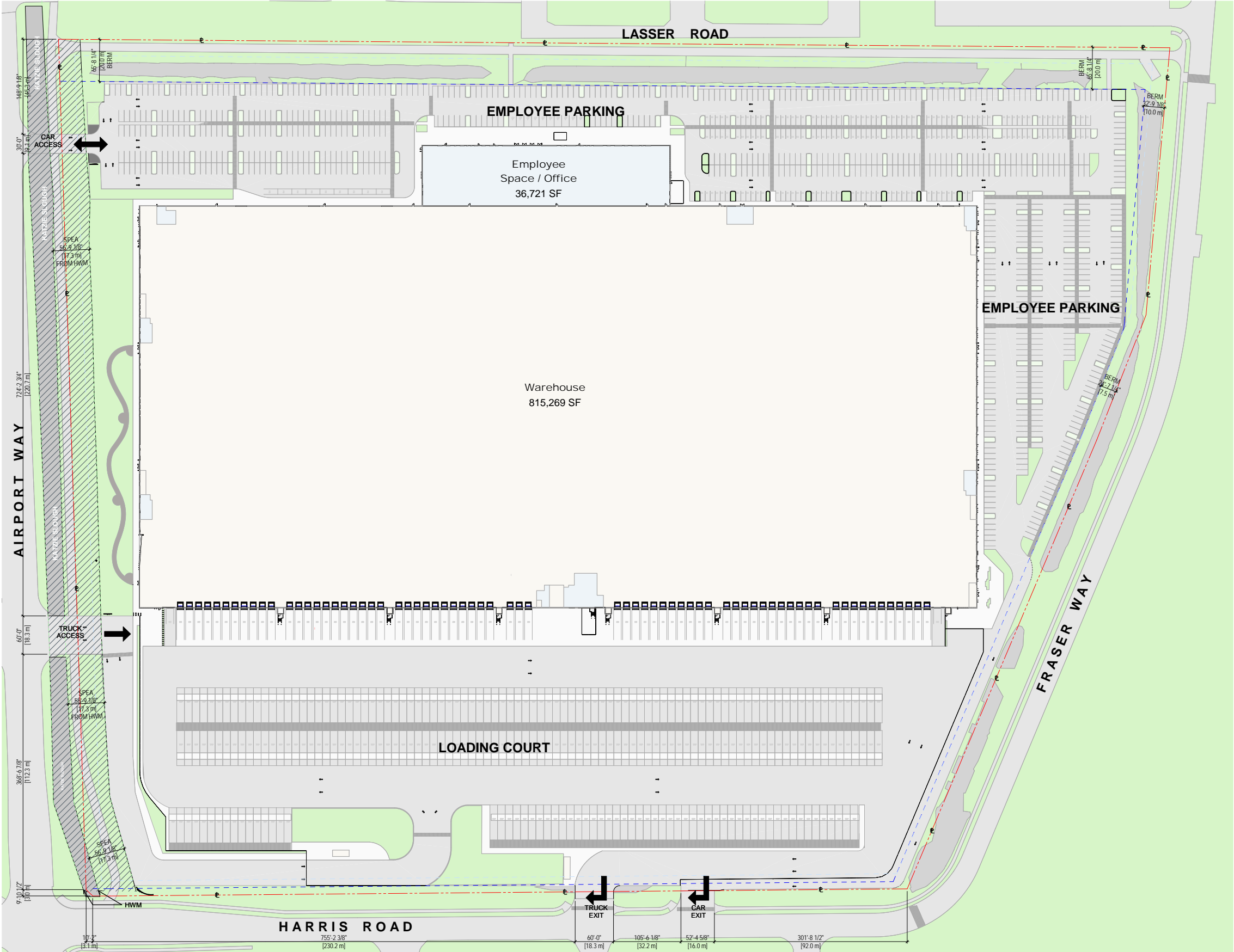
ARCHITECTURAL	
A000	Cover Sheet
A001	Project Data
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A005	Precedents Images
A006	3D Views
A010	Signage Location
A011	Site Signage
A012	Building Signage
A050	Site Plan - Fire Escape Route
A101	Site - Key Plan
A102	Site Plan - NE
A103	Site Plan - SE
A104	Site plan - W
A110	Level 1
A300	Overall Building Elevation
A301	Enlarged North Elevation
A302	Enlarged South Elevation
A303	Enlarged East Elevation
A304	Enlarged East Elevation
A305	Enlarged West Elevation
A306	Enlarged West Elevation
A400	Building Section
A401	Building Section

### PROJECT TEAM

DEVELOPMENT MANAGER	ONNI GROUP
	200 - 1010 Seymour Street, Vancouver, BC P. (604) 260-5009 Robert Vrooman
ARCHITECTS	TKA+D Architecture + Design Inc.
	305 - 1930 Pandora Street, Vancouver, BC P. 604.569.3499 F. 604.569.1394 Craig Taylor, Architect ABC
CIVIL ENGINEER	Binnie
	300 - 4940 Canada Way Burnaby, BC, V5G 4K6 Brad Gilmore
LANDSCAPE ARCHITECT	Durante Kreuk Ltd.
	102, 1637 W 5th Avenue Vancouver BC V6J 1N5, P. 604.684.4611 Peter Krouk

4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit
REV	DATE	DESCRIPTION
SCALE: As indicated	DATE:	DRAWN: Author
PROJECT NUMBER		18038
FILE PATH:		PLOT DATE: 2021-05-03 8:38:01 AM
TKA+D		
ARCHITECTURE + DESIGN INC.		
305 - 1930 PANDORA STREET, VANCOUVER, BC V6J 1N5		
GEBP - PH4 - Build to Suit		
11208 Harris Road, Pitt Meadows		
Project Data		
PLOT DATE: 2021-05-03 8:38:01 AM		
A001		





1 Context Plan  
1" = 60'-0"

3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

REV	DATE	DESCRIPTION

SCALE: 1" = 60'-0"      DATE:      DRAWN: Author

PROJECT NUMBER: 18038

FILE PATH:      PLOT DATE: 2021-05-03 8:38:25 AM

**TKA+D**  
ARCHITECTURE + DESIGN INC.  
208 - 1000 HANCOCK STREET - VANCOUVER, BC, V6L 4P6

**GEBP - PH4 - Build to Suit**  
11208 Harris Road,  
Pitt Meadows

**Context plan**

PLOT DATE: 2021-05-03 8:38:25 AM

**A003**

Site organization

Situated at the intersection of Harris Road and Airport Way, West of the Golden Ears Bridge, Phase 4 of the Golden Ears Business Park consists of one single tenant industrial building. It is designed in line with the requirements of the Pitt Meadows Design Guidelines for the site as well as the needs of the future tenants. The building will be constructed of high-quality tilt up concrete panels that celebrate a carefully articulated and aesthetically pleasing façade.

The building is setback from the property line to lower its impact on the neighboring street and the pedestrian path. The north side of the site is adjacent to a Streamside Protection and Enhancement Area (SPEA) associated with Katzie slough. A 20m landscape area is provided on the east side of the site including a pedestrian path connecting Airport Way to the existing trails on Fraser Way, and local, residential streets. On the eastern side of the site is a large, heavily planted employee parking area, with trees planted at a density of one for every 6 stalls.

Architecture and Massing

TKA+D Architecture + Design inc., who have been engaged as architects for this project, are renowned for creating high quality public, residential, commercial, and industrial architecture. They have received many design awards for their work including the Architectural Institute of BC's prestigious Innovation award as well as 4 UDI awards and 3 NAIOF awards for their industrial projects.

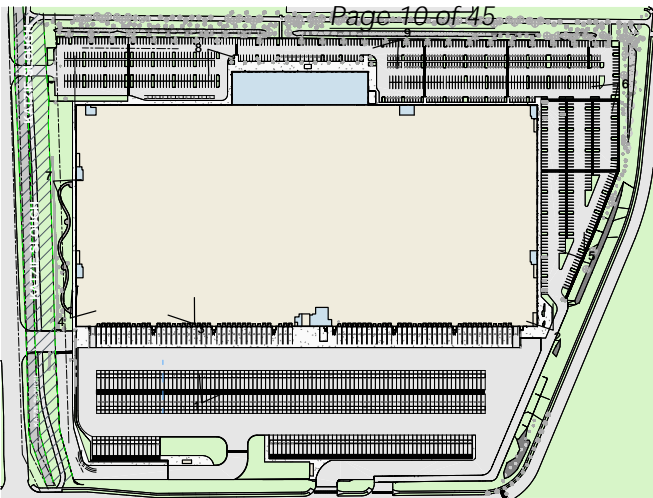
Given the proximity of the development to the single-family residential neighborhood to the east of the site, careful attention has been given to the façade treatment, articulation and massing of the building to ensure an aesthetically pleasing and human-scale streetscape is maintained.

All corners are punctuated with significant areas of glazing, to bring more natural light to the building, as well as facilitating a high degree of flexibility of the building to accommodate a variety of tenants sizes. The Design also incorporate a significant amount of faux wood elements into the soffits, vertical fins and canopies at the corners.

The West façades is designed in modules featuring dock and grade loading doors, and high level windows. A carefully composed arrangement of architectural reveals and panel joints help to further articulate this massing to ensure an aesthetically pleasing and human-scale streetscape is maintained.

Additionally, a projecting "pop out" form is located at each exit to bring natural light into the building, as well as to allow for safe egress. The creation of these smaller elements also visually break down in the height of the building, and to strengthen the different rhythms of this facade.

Long lengths of walls on the north and south sides are punctuated by a rhythm of glazing and solid tilt-up panels, variations in color and wall height, recessed entries, and canopies.



Location Plan



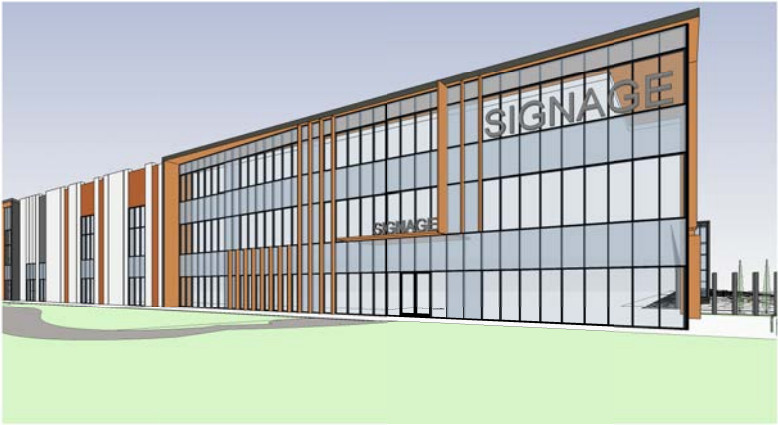
View 1



View 2



View 3



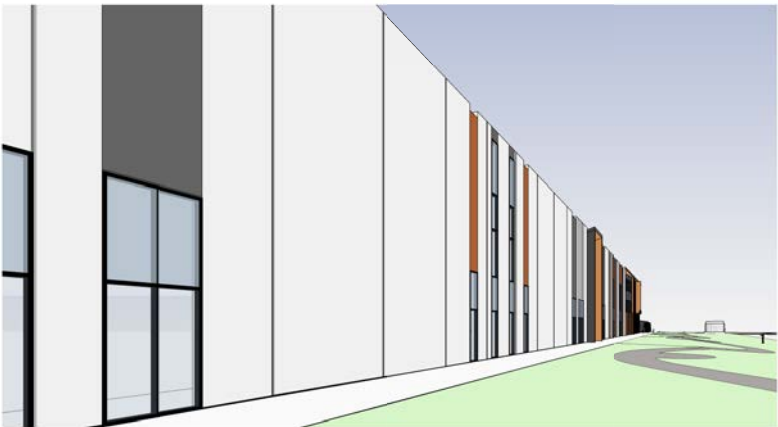
View 4



View 5



View 6



View 7



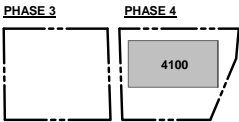
View 8



View 9



KEYPLAN 1







① NW Corner



② NE Corner



③ SW Corner



④ SE Corner



⑤ NE Office Corner



⑥ SE Office Corner  
-196-





42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION

SCALE:

DATE:

DRAWN: Author

PROJECT NUMBER

18038

FILE PATH:

PLOT DATE: 2021-05-03 8:40:17 AM

TKA+D

ARCHITECTURE + DESIGN INC

200 - 1000 SANDRA STREET - VANCOUVER, BC, V6E 4P5 (P. 604) 689-9901

GEBP - PH4 - Build to Suit

11208 Harris Road, Pitt Meadows

Rendering Image

PLOT DATE: 2021-05-03 8:40:17 AM

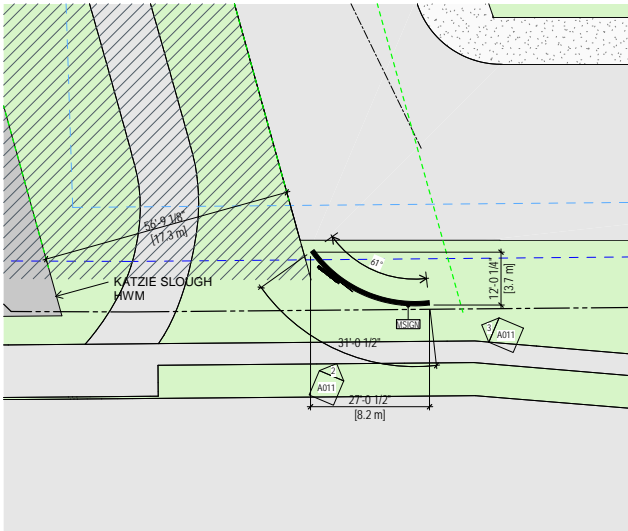
A007



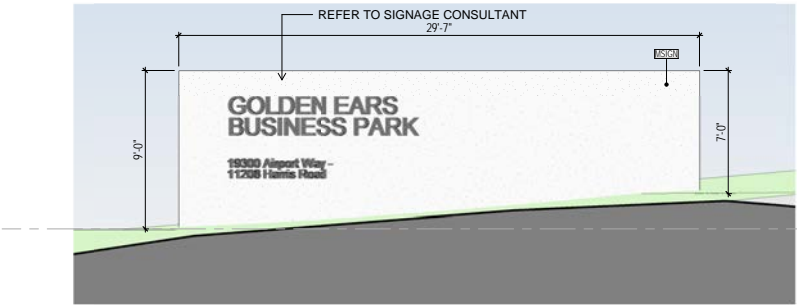




Signage Legend	
SYMBOL	DESCRIPTION
CSIGN	CANOPY SIGN
DSIGN	DIRECTIONAL SIGN
FSIGN	FASCIA SIGN
MSIGN	MONUMENT SIGN



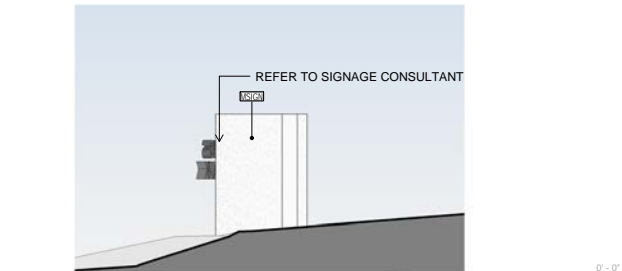
① Monument Sign Plan  
1/16" = 1'-0"



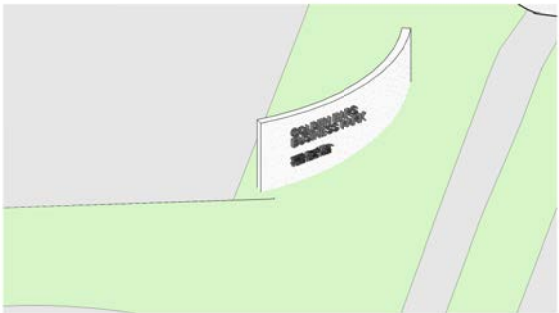
② Monument Sign Elevation - Front  
1/4" = 1'-0"



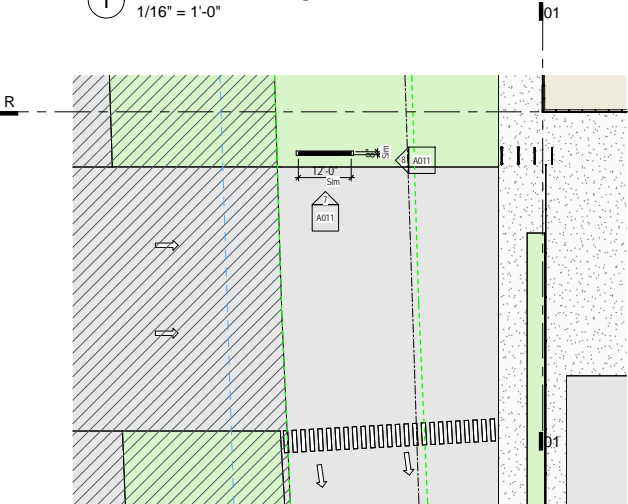
Precedent of Existing Monument Sign for Golden Ears Business Parks Phase 1 - Cnr of Harris Road and Airport Way



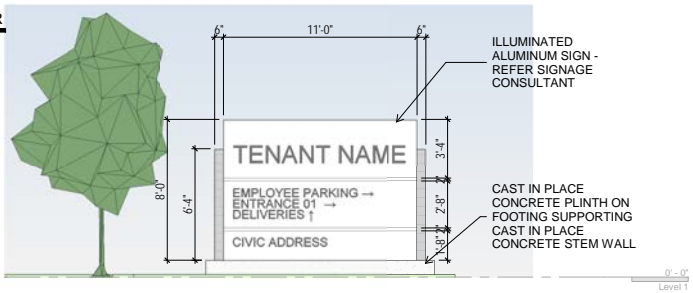
③ Monument Sign Elevation - Side  
1/4" = 1'-0"



④ Monument Sign - 3D View



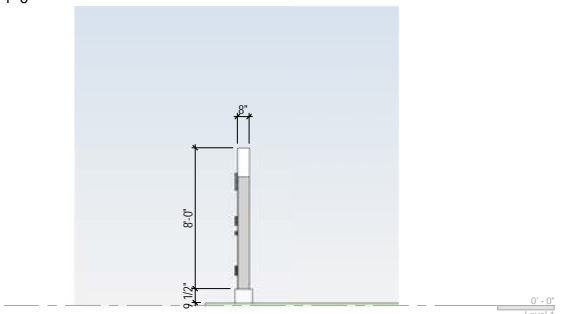
⑤ Directional Monument Sign - NW  
ENTRANCE  
1/16" = 1'-0"



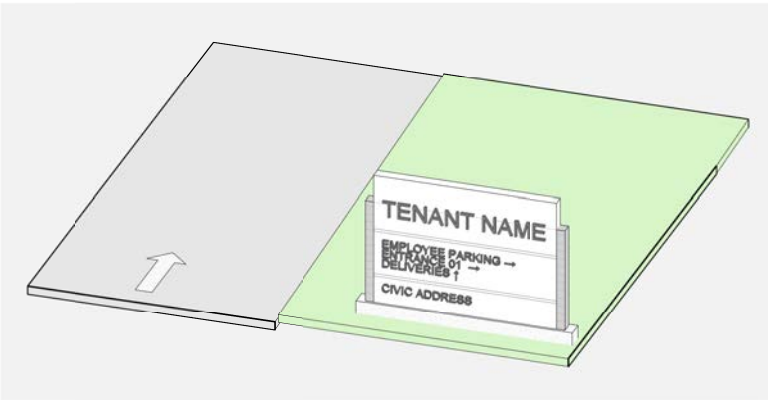
⑦ Directional Monument Sign - Front  
Elevation  
1/4" = 1'-0"



⑥ Directional Monument Sign- NE ENTRANCE  
1/16" = 1'-0"



⑧ Directional Monument Sign - Side Elevation  
1/4" = 1'-0"

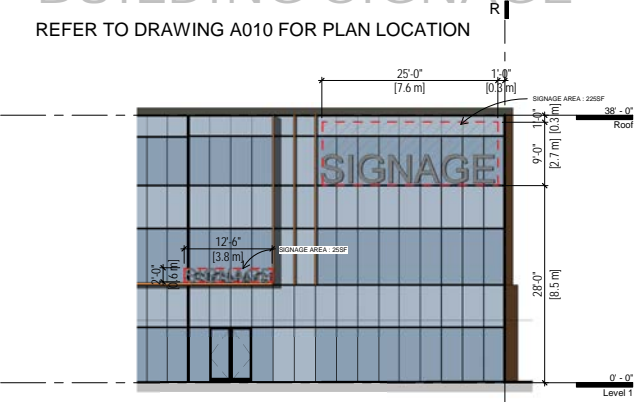


⑨ Directional Monument sign - 3D View

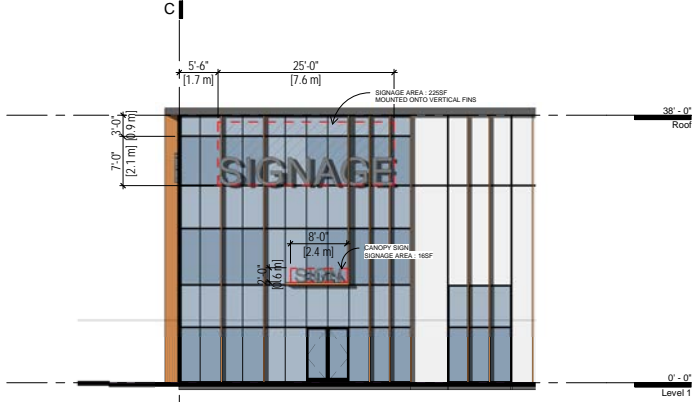


BUILDING SIGNAGE

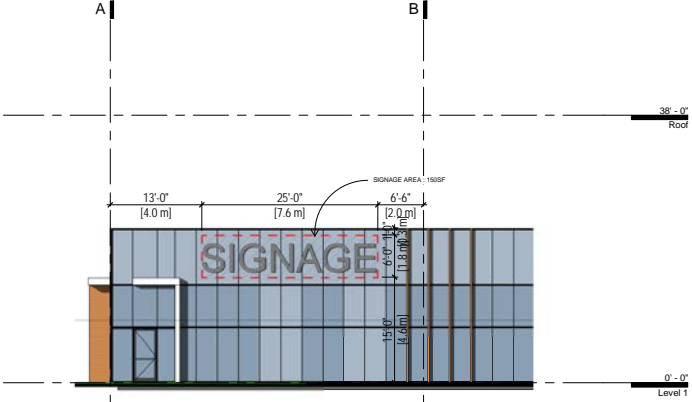
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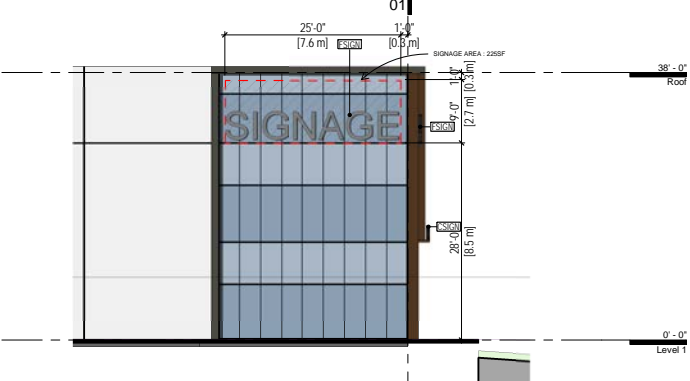
① Signage N Elevation - GL.R  
1" = 10'-0"



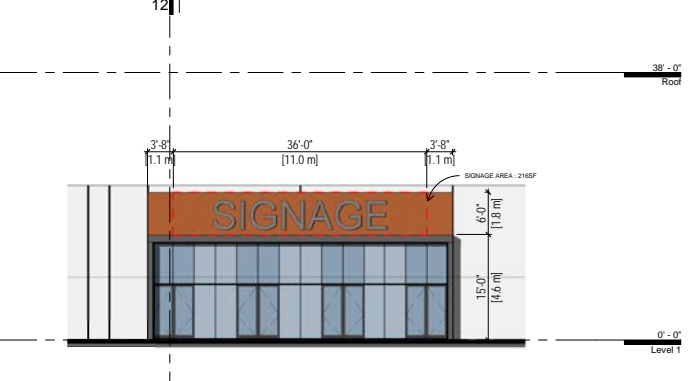
② Signage N Elevation - GL.C  
1" = 10'-0"



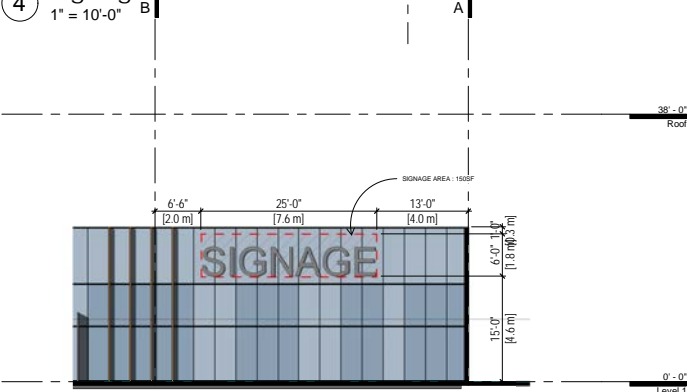
③ Signage N Elevation - Office  
1" = 10'-0"



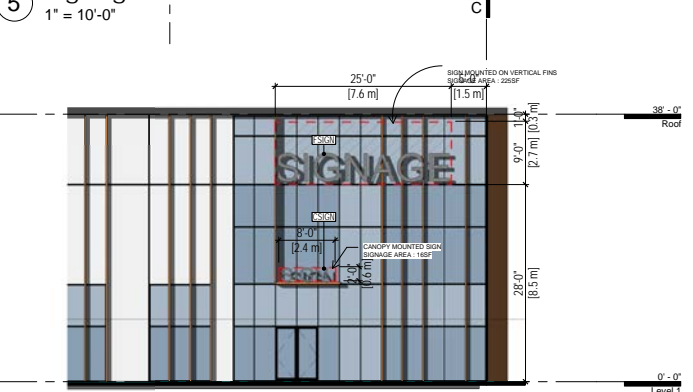
④ Signage E Elevation - GL.01  
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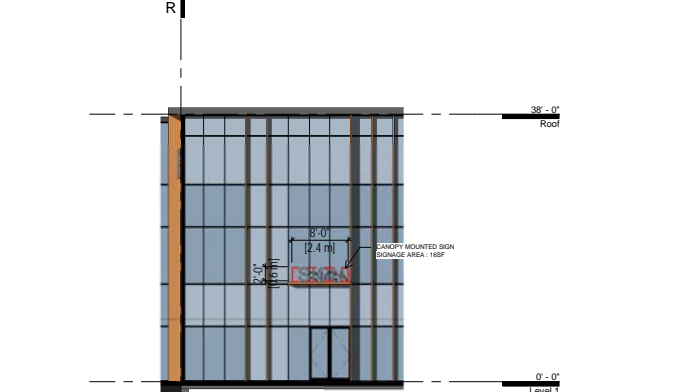
⑤ Signage E Elevation - Office  
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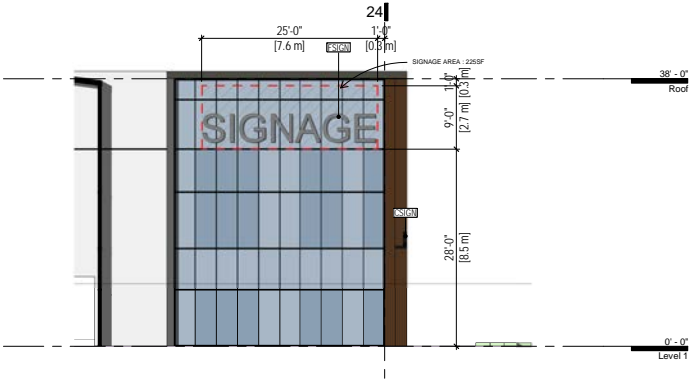
⑥ Signage S Elevation - Office  
1" = 10'-0"



⑦ Signage S Elevation - GL.C  
1" = 10'-0"



⑧ Signage S Elevation - GL.R  
1" = 10'-0"

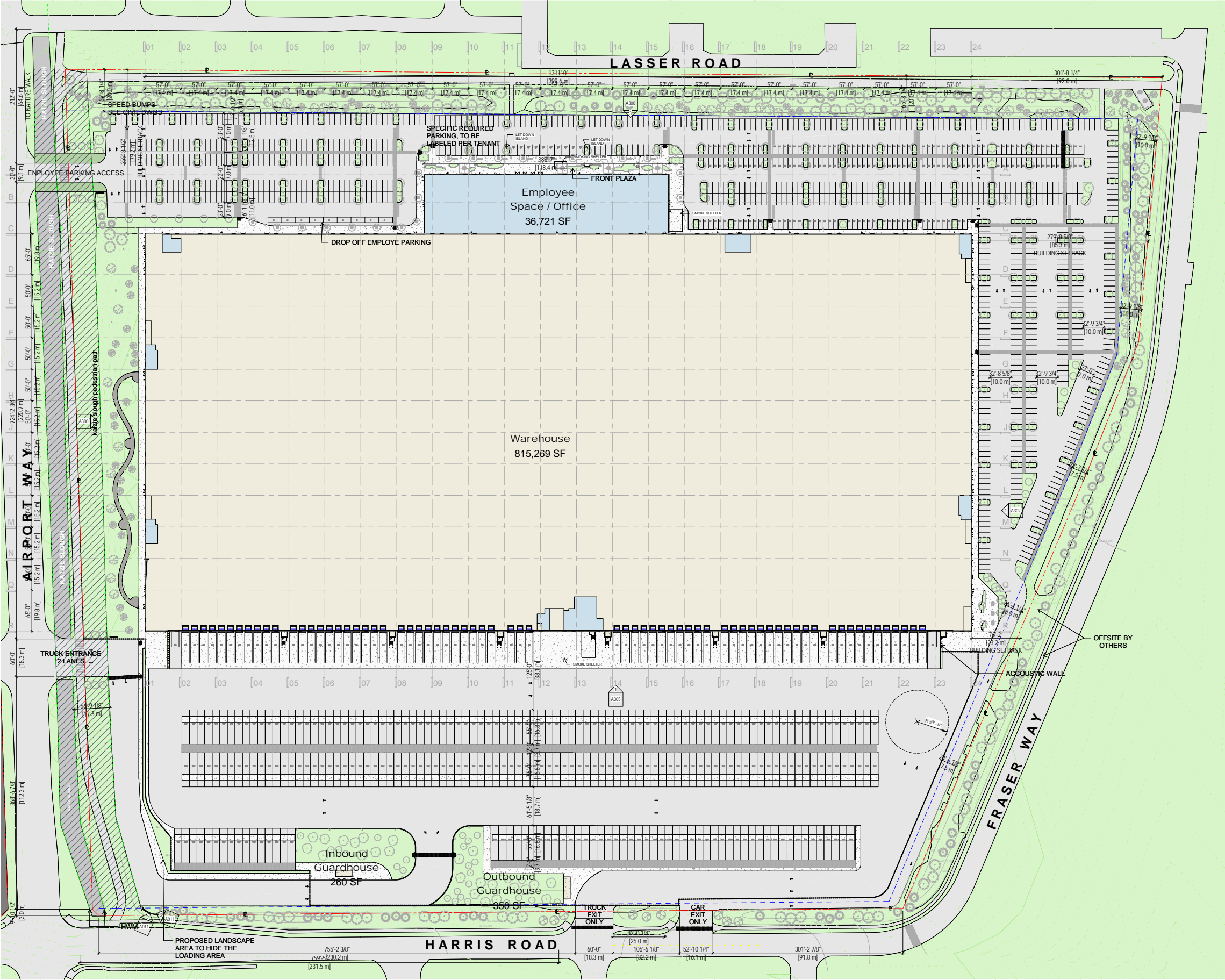
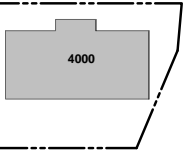


⑨ Signage W Elevation - GL.24  
1" = 10'-0"



KEYPLAN

PHASE 4



4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit
REV	DATE	DESCRIPTION
SCALE	As indicated	DATE: 2021-05-03
PROJECT NUMBER	18038	DRAWN: Author
FILE PATH		PLOT DATE: 2021-05-03 14:23:00

**TKA+D**  
ARCHITECTURE + DESIGN INC.  
200 - 1000 HARRIS STREET - VANCOUVER, BC, V6P 6P6

**GEBP - PH4 - Build to Suit**  
11208 Harris Road,  
Pitt Meadows

**Site - Key Plan**

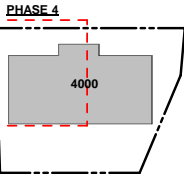
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**A101**

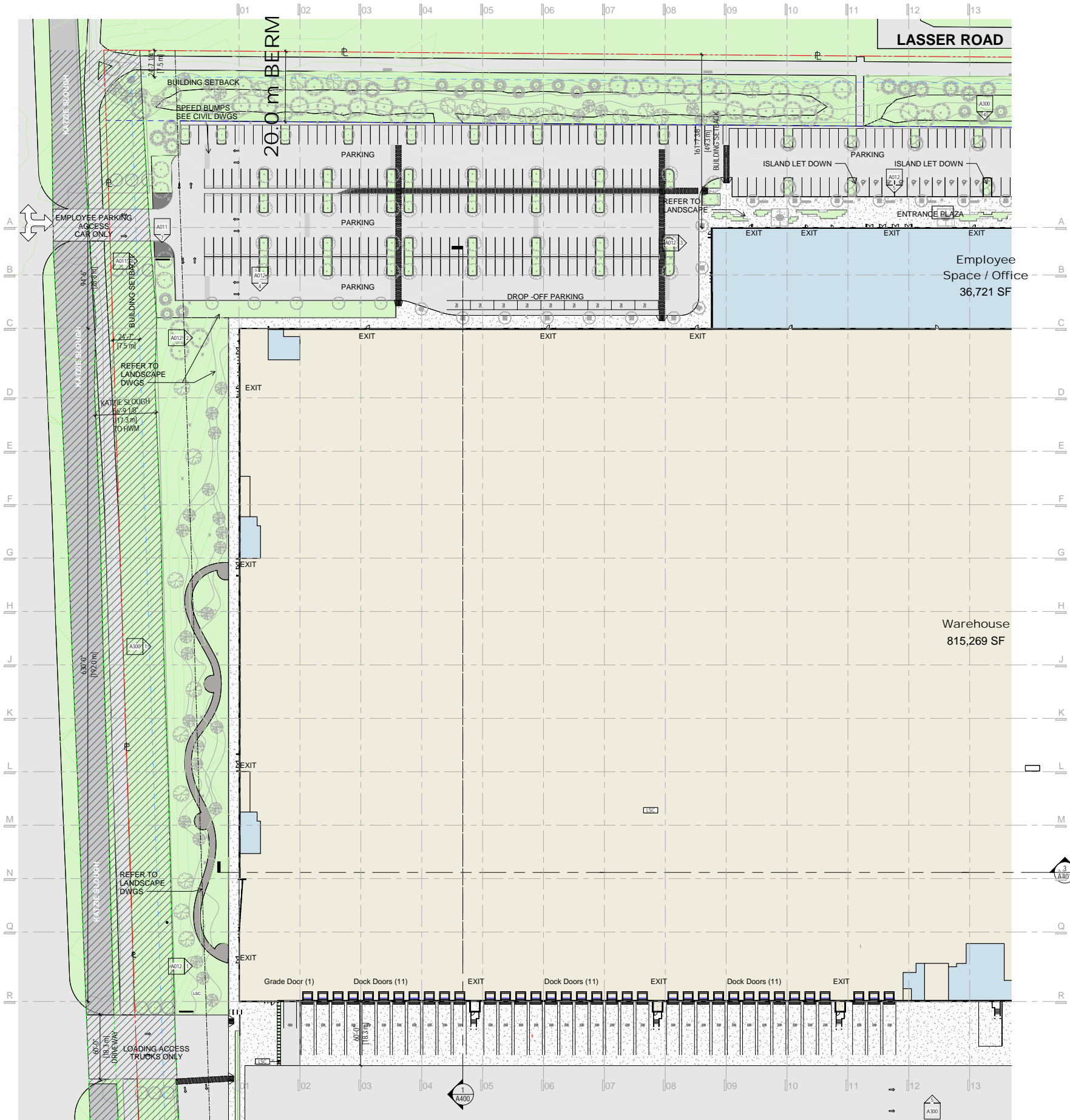
1 Overall - Site plan  
1" = 60'-0"



KEYPLAN



KEYNOTE LEGEND	
TAG	DESCRIPTION
LSC	LANDSCAPING: SEE LANDSCAPE DETAILS



**SITE LEGEND**

- PROPERTY LINE
- BERM SETBACK
- HIGH WATER MARK - KATZIE SLOUGH
- BUILDING SETBACK
- KATZIE SLOUGH
- SPEA

**PARKING LEGEND**

Symbol	Dimensions	Description
SC	15'-9" (4.8 m) x 7'-10" (2.4 m)	SMALL CAR PARK
RC	18'-1" (5.5 m) x 8'-6" (2.6 m)	REGULAR CAR PARK
AC	18'-1" (5.5 m) x 11'-10" (3.6 m)	ACCESSIBLE CAR PARK
PC	22'-0" (6.7 m) x 8'-7" (2.62 m)	PARALLEL CAR PARK
TRP	55'-0" (16.76 m) x 12'-0" (3.66 m)	TRAILER PARKING
DB	59'-0" (18.24 m) x 12'-0" (3.66 m)	DOCK LOADING BAY
GB	25'-0" (7.62 m) x 12'-0" (3.66 m)	GRADE LOADING BAY

4 2021-05-03 Re-issued for Development Permit  
3 2021-03-05 Re-issued for Development Permit  
2 2020-05-08 Re-issued for Development Permit  
1 2020-03-10 Issued for Development Permit

REV	DATE	DESCRIPTION
4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

SCALE: As indicated DATE: DRAWN: Author  
PROJECT NUMBER: 18038  
FILE PATH: PLOT DATE: 2021-05-03 3:46:33 PM

**TKA+D**  
ARCHITECTURE + DESIGN INC.  
200-1100 HARRIS STREET - VANCOUVER, BC, V6P 6P6

**GEBP - PH4 - Build to Suit**  
11208 Harris Road,  
Pitt Meadows

**Site Plan - NE**

PLOT DATE: 2021-05-03 3:46:33 PM

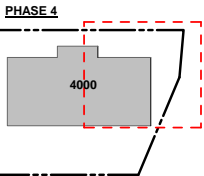
**A102**

1 Site Plan - North East  
1" = 40'-0"

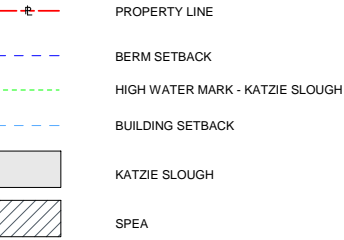




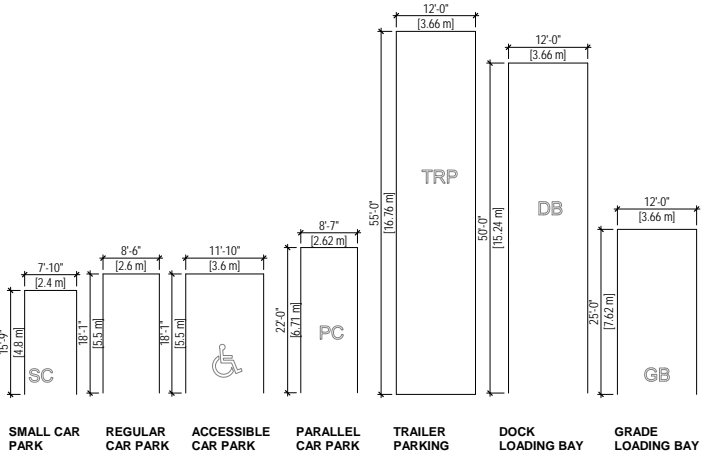
KEYPLAN



KEYNOTE LEGEND	
TAG	DESCRIPTION
FNC1	FENCE TYPE 1: CHAIN LINK FENCING; C/W GATES AND HARDWARE, SEE SECTIONS & ELEVATIONS TO IDENTIFY HEIGHT, GALVANIZED.
LSC	LANDSCAPING; SEE LANDSCAPE DETAILS



SITE LEGEND



42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION
4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

SCALE: As indicatedDATE:18038DRAWN: AuthorPROJECT NUMBER:18038FILE PATH:18038PLOT DATE:18038

TKA+D

ARCHITECTURE + DESIGN INC

200-1100 HARRIS STREET - VANCOUVER, BC, V6P 6P6

GEBP - PH4 - Build to Suit

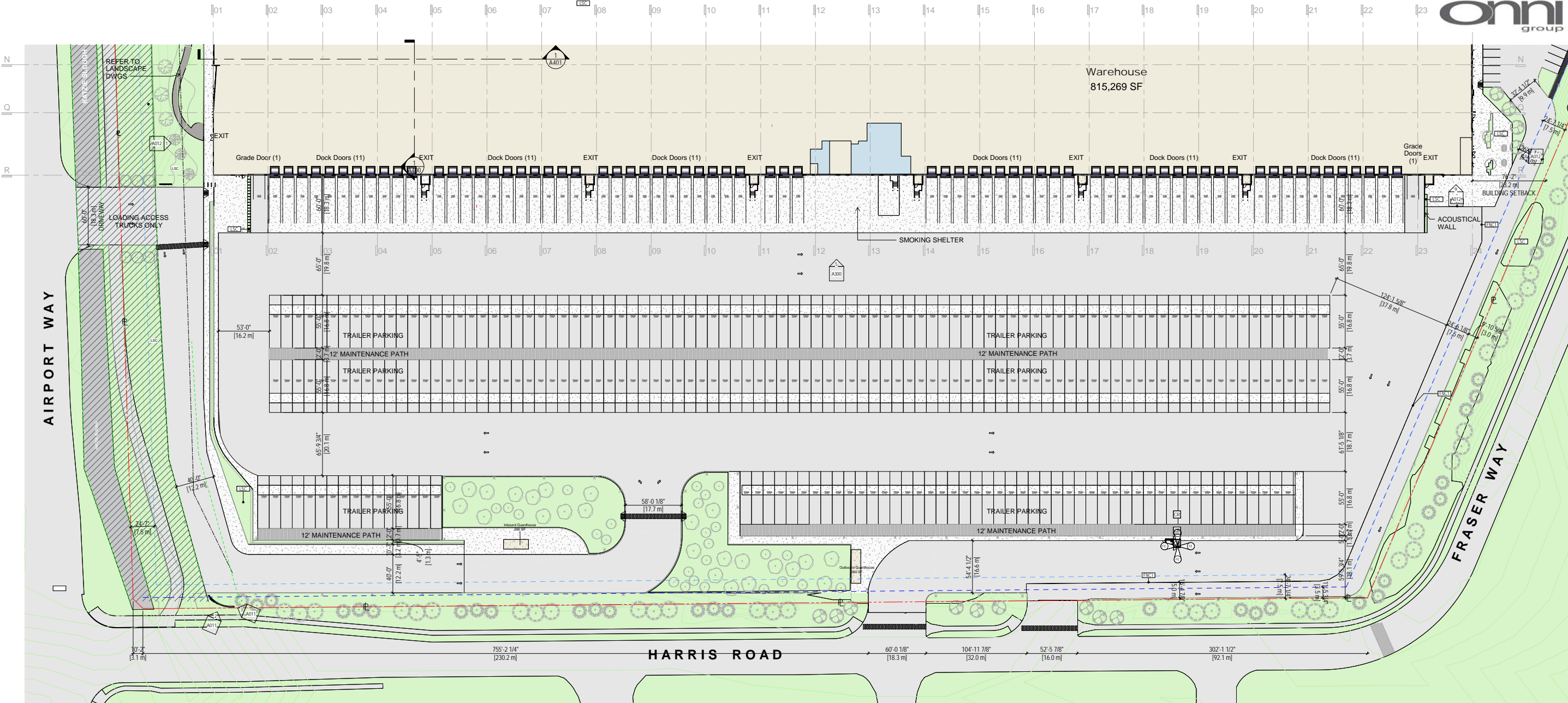
11208 Harris Road, Pitt Meadows

Site Plan - SE

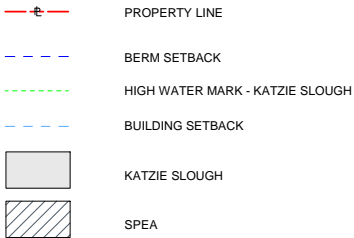
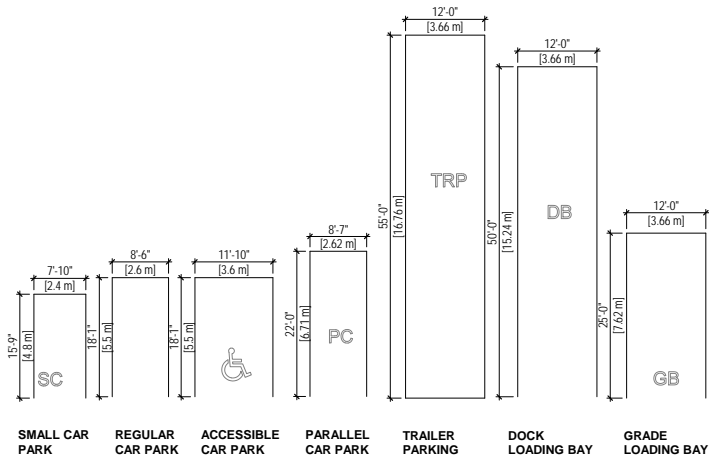
PLOT DATE: 2021-05-03 3:46:58 PM

A103

1 Site Plan -South East  
1" = 40'-0"



1 Site plan - West  
1" = 40'-0"

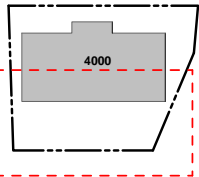


KEYNOTE LEGEND

TAG	DESCRIPTION
FNC1	FENCE TYPE 1: CHAIN LINK FENCING; C/W GATES AND HARDWARE, SEE SECTIONS & ELEVATIONS TO IDENTIFY HEIGHT, GALVANIZED.
LSC	LANDSCAPING, SEE LANDSCAPE DETAILS

KEYPLAN

PHASE 4



2	2021-05-03	Re-issued for Development Permit		
1	2021-03-05	Re-issued for Development Permit		
REV	DATE	DESCRIPTION		
SCALE:	As indicated	DATE:	DRAWN:	Author
PROJECT NUMBER	18038			
FILE PATH:			PLOT DATE:	2021-05-03 8:42:58 AM

TKA+D  
ARCHITECTURE + DESIGN INC.  
305-1100 HARRIS ROAD, PITT MEADOWS, BC V3Y 6P6

GEBP - PH4 - Build to Suit  
11208 Harris Road,  
Pitt Meadows

Site plan - W

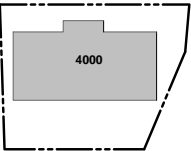
PLOT DATE: 2021-05-03 8:42:58 AM

A104

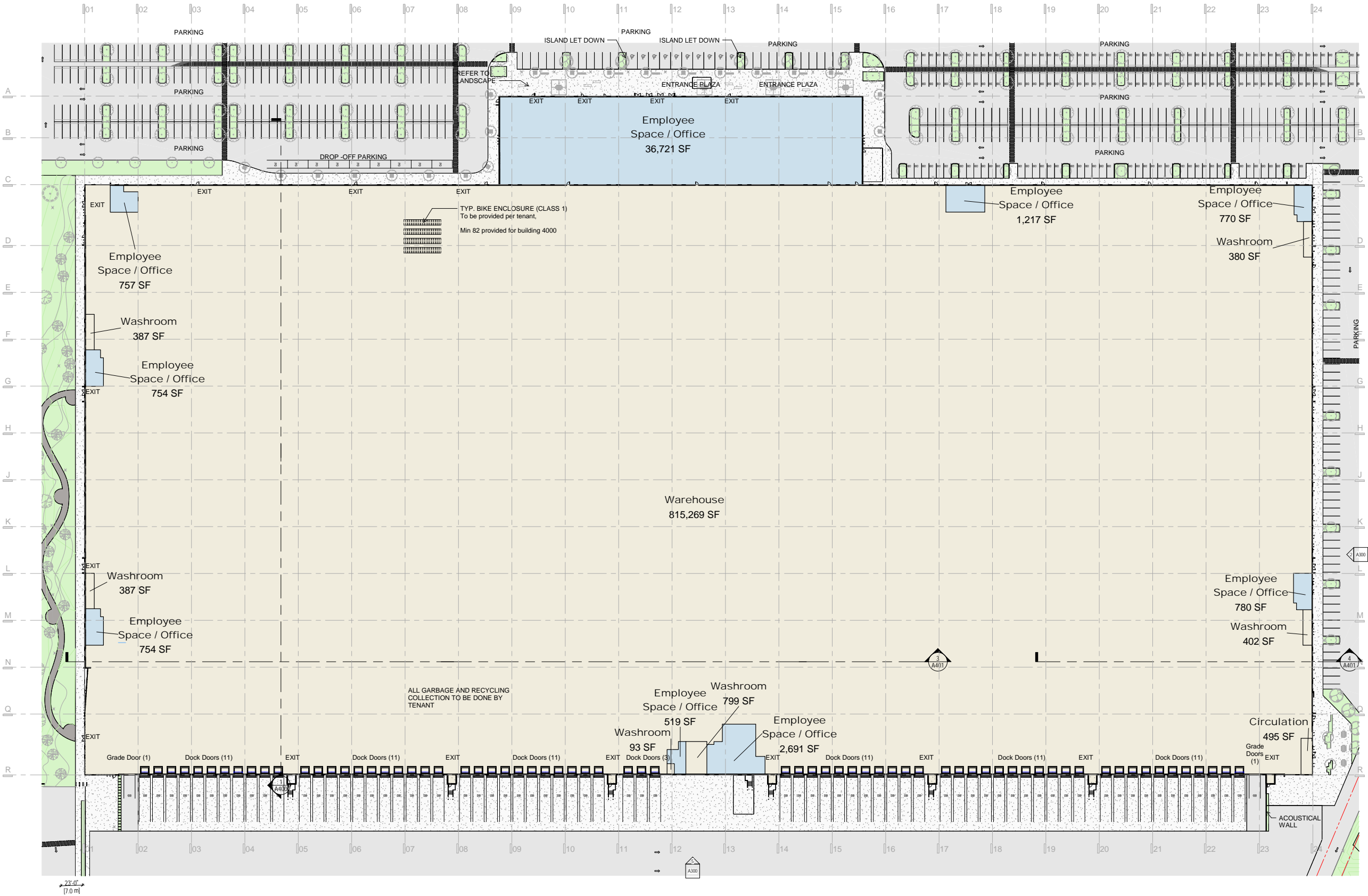


KEYPLAN

PHASE 4



KEYNOTE LEGEND  
TAG DESCRIPTION



4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit
REV	DATE	DESCRIPTION
SCALE: As indicated	DATE:	DRAWN: Author
PROJECT NUMBER	18038	
FILE PATH:	PLOT DATE:	2021-05-03 14:37:00

TKA+D  
ARCHITECTURE + DESIGN INC.  
200-1000 HARRIS STREET - VANCOUVER, BC V6L 4C7 P 604 681 9400

GEBP - PH4 - Build to Suit  
11208 Harris Road,  
Pitt Meadows

Level 1

PLOT DATE: 2021-05-03 8:43:11 AM

A110

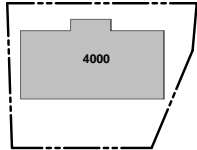
1 Level 1 .  
1" = 40'-0"





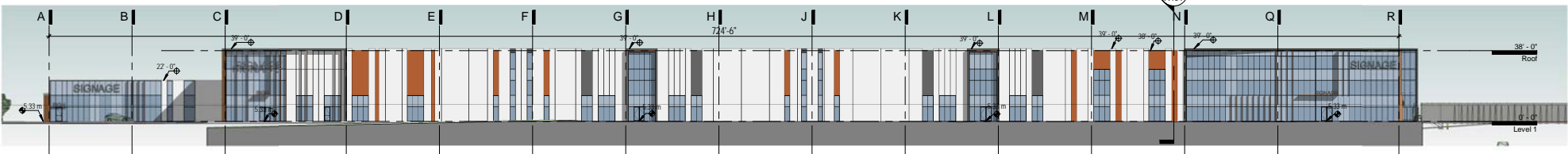
KEYPLAN

PHASE 4



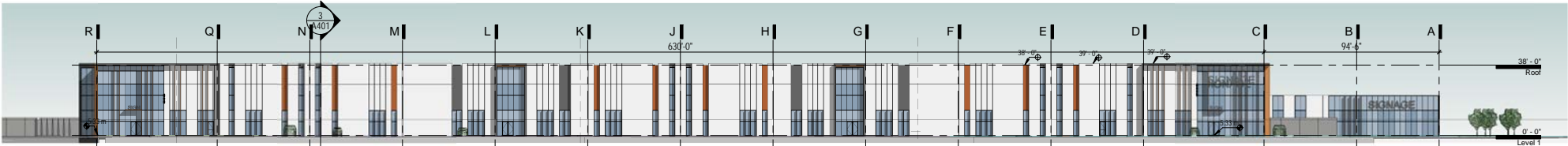
Keyplan 2- PH4

KEYNOTE LEGEND	
TAG	DESCRIPTION



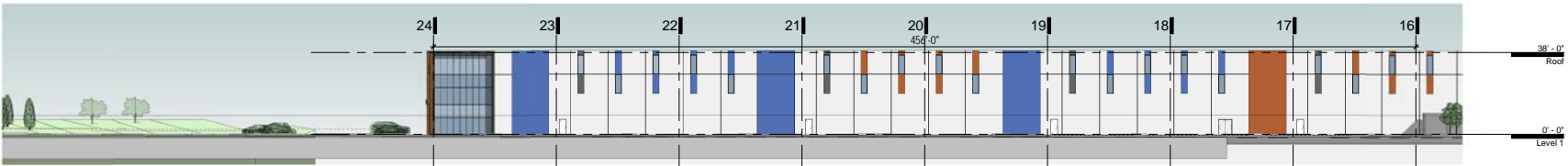
① North  
1" = 30'-0"

\* Airport Way (north) elevation complies with City of Pitt Meadows's required 50% Glazing Ratio at Ground Floor Level.



② South  
1" = 30'-0"

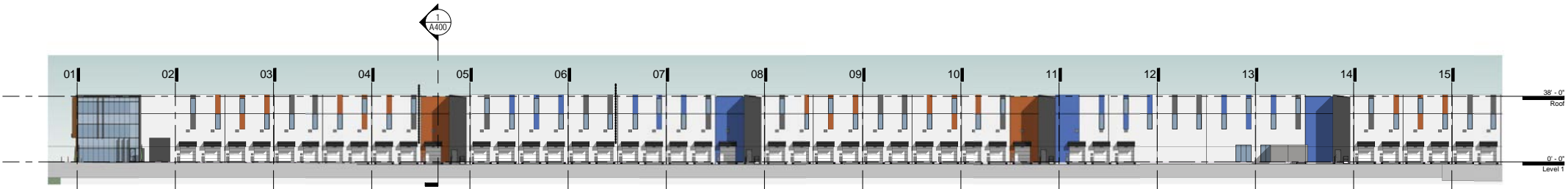
\* Fraser Way (south) elevation provides 48% Glazing Ratio at Ground Floor Level.



③ East - GL 24-13  
1" = 30'-0"



④ East - 16-01  
1" = 30'-0"



⑤ West - 01-15  
1" = 30'-0"

\* Harris Road (west) elevation provides 8.2 % Glazing Ratio at Ground Floor Level.



⑥ West - 15-24  
1" = 30'-0"

32021-03-05 Re-issued for Development Permit

22020-05-08 Re-issued for Development Permit

12020-03-10 Issued for Development Permit

REV	DATE	DESCRIPTION
1	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
3	2020-03-10	Issued for Development Permit

SCALE: As indicated

DATE: 2021-05-03

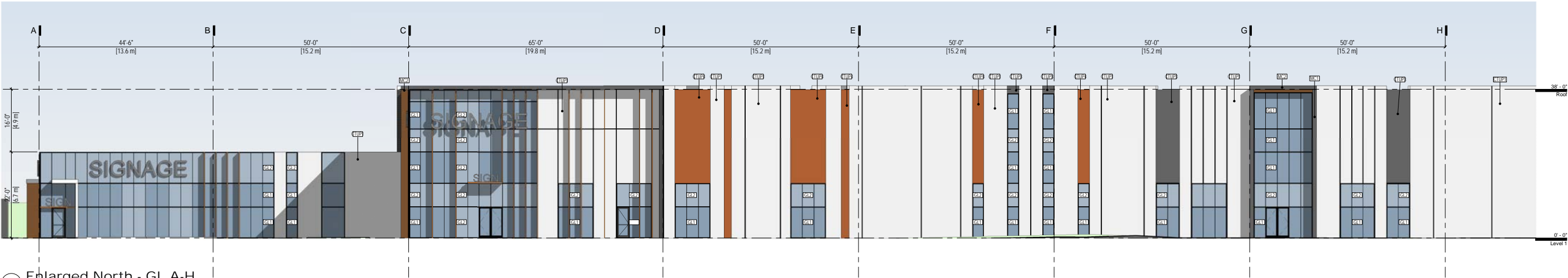
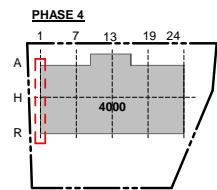
DRAWN: Author

PROJECT NUMBER: 18038

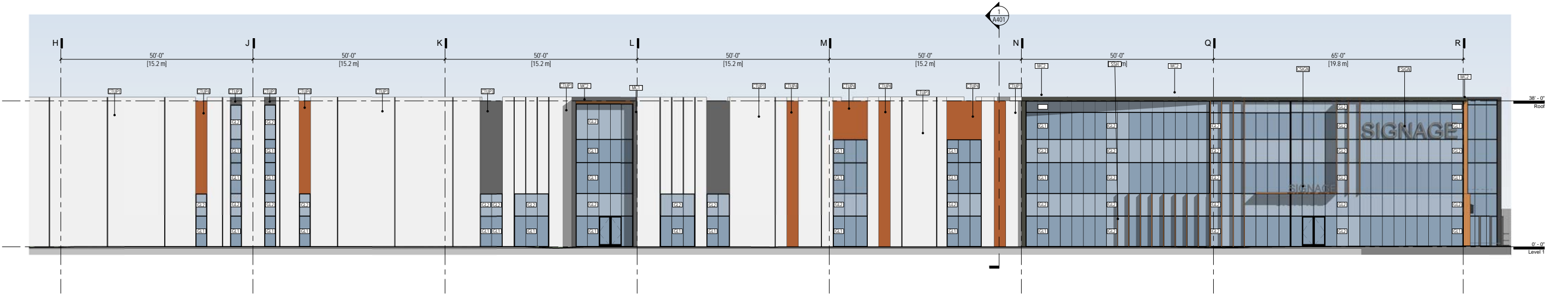
FILE PATH: 2021-05-03

PLOT DATE: 8:43:31 AM

TKA+D  
ARCHITECTURE + DESIGN INC  
205 - 1100 HARRIS STREET - VANCOUVER, BC, V6E 4P9  
GEBP - PH4 - Build to Suit  
11208 Harris Road,  
Pitt Meadows  
Overall Building Elevation  
PLOT DATE: 2021-05-03 8:43:31 AM  
A300



1 Enlarged North - GL.A-H  
1" = 10'-0"



2 Enlarged North - GL.H-R  
1" = 10'-0"

KEYNOTE LEGEND	
TAG	DESCRIPTION
CSIGN	CANOPY SIGN
CTUP1	CONCRETE TILT-UP; PAINT FINISH, P1-LIGHT GREY
CTUP3	CONCRETE TILT-UP; PAINT FINISH, P3-CHARCOAL
CTUP4	CONCRETE - TILT UP - PAINTED P4 - ORANGE
FSIGN	FASCIA SIGN
GL1	VISION GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
GL2	SPANDREL GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
MC1	METAL CLADDING - CHARCOAL
MC2	METAL CLADDING - FAUX WOOD - TYP. LONGBOARD
SSH	SUNSHADE

42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION
SCALE: As indicated	DATE:	DRAWN: Author
PROJECT NUMBER	18038	
FILE PATH:	2021-05-03 8:43:42 AM	

TKA+D

ARCHITECTURE + DESIGN, INC.

200 - 1000 SANDRA STREET - VANCOUVER, BC, V6P 6P9

GEBP - PH4 - Build to Suit

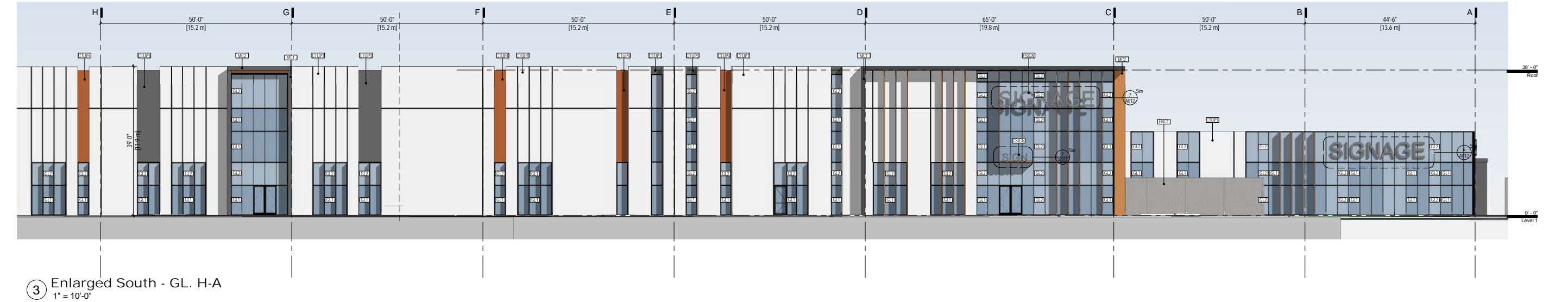
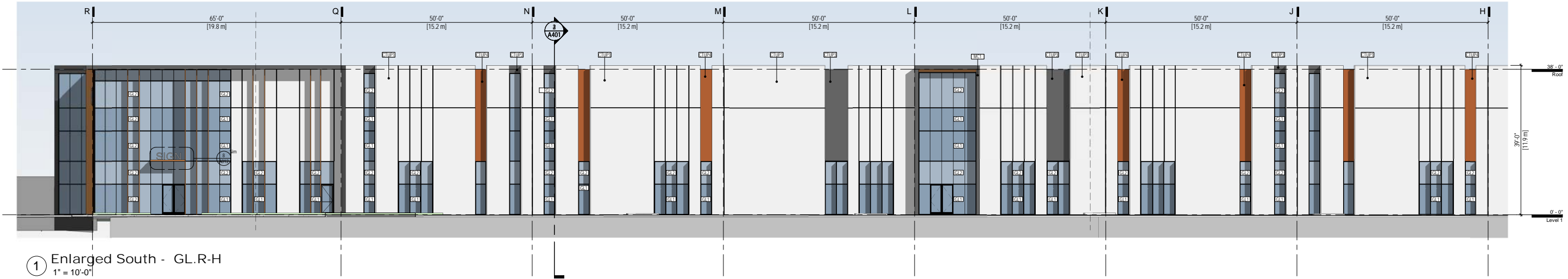
11208 Harris Road, Pitt Meadows

Enlarged North Elevation

PLOT DATE: 2021-05-03 8:43:42 AM

A301





KEYNOTE LEGEND

TAG	DESCRIPTION
CSIGN	CANOPY SIGN
CTUP1	CONCRETE TILT-UP; PAINT FINISH, P1-LIGHT GREY
CTUP3	CONCRETE TILT-UP; PAINT FINISH, P3-CHARCOAL
CTUP4	CONCRETE - TILT UP - PAINTED P4 - ORANGE
FNC1	FENCE TYPE 1: CHAIN LINK FENCING; C/W GATES AND HARDWARE, SEE SECTIONS & ELEVATIONS TO IDENTIFY HEIGHT, GALVANIZED.
FSIGN	FASCIA SIGN
GL1	VISION GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
GL2	SPANDREL GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
MC1	METAL CLADDING - CHARCOAL
MC2	METAL CLADDING - FAUX WOOD - TYP. LONGBOARD

4 2021-05-03 Re-issued for Development Permit  
3 2021-03-05 Re-issued for Development Permit  
2 2020-05-08 Re-issued for Development Permit  
1 2020-03-10 Issued for Development Permit

REV	DATE	DESCRIPTION
1	2021-05-03	Re-issued for Development Permit

SCALE: 1" = 10'-0" DATE: DRAWN: Author  
PROJECT NUMBER 18038  
FILE PATH: PLOT DATE: 2021-05-03 8:43:52 AM

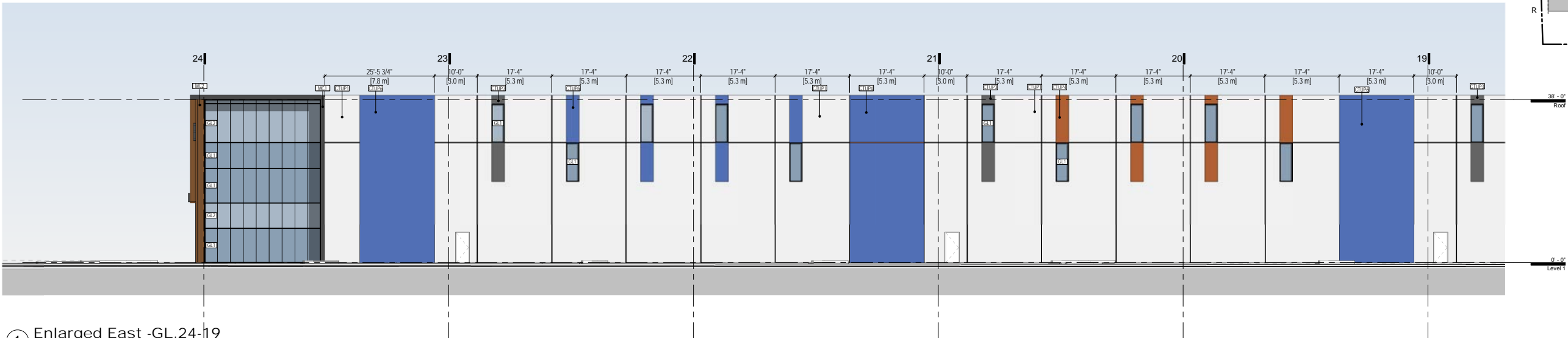
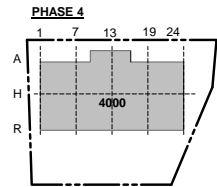
TKA+D  
ARCHITECTURE + DESIGN INC.  
300 - 1000 HARRIS STREET, VANCOUVER, BC V6L 4G7 P 604.689.9900

GEBP - PH4 - Build to Suit  
11208 Harris Road,  
Pitt Meadows

Enlarged South Elevation

PLOT DATE: 2021-05-03 8:43:52 AM

A302



① Enlarged East -GL.24-19  
1" = 10'-0"



② Enlarged East -GL.19-13  
1" = 10'-0"

KEYNOTE LEGEND	
TAG	DESCRIPTION
CTUP1	CONCRETE TILT-UP; PAINT FINISH, P1-LIGHT GREY
CTUP3	CONCRETE TILT-UP; PAINT FINISH, P3-CHARCOAL
CTUP4	CONCRETE - TILT UP - PAINTED P4 - ORANGE
CTUP6	CONCRETE - TILT UP - PAINTED P6 - BLUE
GL1	VISION GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
GL2	SPANDREL GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
MC1	METAL CLADDING - CHARCOAL
MC2	METAL CLADDING - FAUX WOOD - TYP. LONGBOARD

42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION
SCALE: As indicated	DATE:	DRAWN: Author
PROJECT NUMBER	18038	
FILE PATH:	PLOT DATE:	2021-05-03 8:43:59 AM

TKA+D

ARCHITECTURE + DESIGN INC

205 - 1001 HANCOCK STREET - VANCOUVER, B.C. V6E 4P9

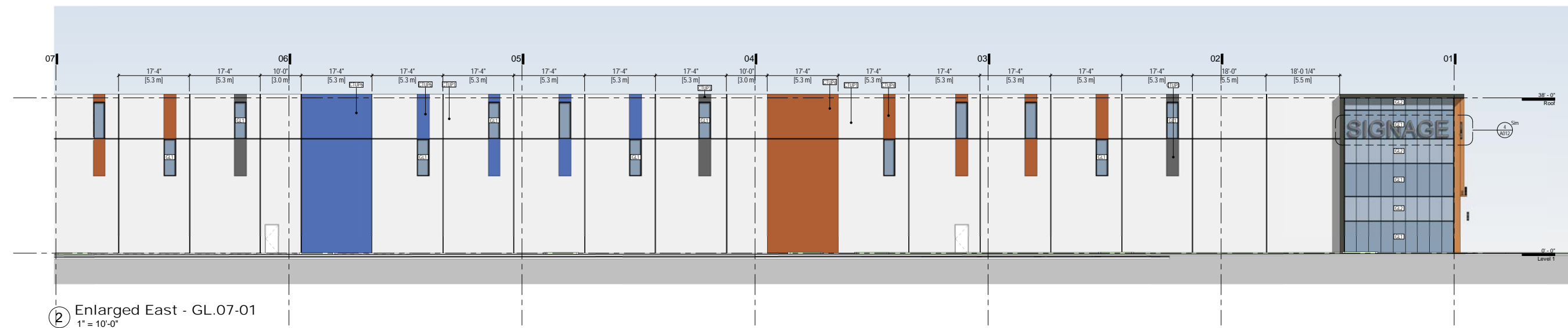
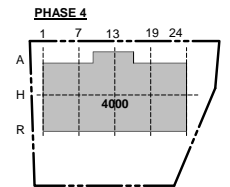
GEBP - PH4 - Build to Suit

11208 Harris Road, Pitt Meadows

Enlarged East Elevation

PLOT DATE: 2021-05-03 8:43:59 AM

A303



### KEYNOTE LEGEND

TAG	DESCRIPTION
CTUP1	CONCRETE TILT-UP; PAINT FINISH, P1-LIGHT GREY
CTUP3	CONCRETE TILT-UP; PAINT FINISH, P3-CHARCOAL
CTUP4	CONCRETE - TILT UP - PAINTED P4 - ORANGE
CTUP6	CONCRETE - TILT UP - PAINTED P6 - BLUE
GL1	VISION GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
GL2	SPANDREL GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE

4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

REV	DATE	DESCRIPTION
SCALE: As indicated		DATE:
PROJECT NUMBER		DRAWN: Author
		18038
FILE PATH:		PLOT DATE: 2021-05-03 8:44:06 AM

TKA+D  
ARCHITECTURE + DESIGN INC  
305 / 1210 PANDORA STREET - VANCOUVER - V6L 0C7 P 604 569 3489

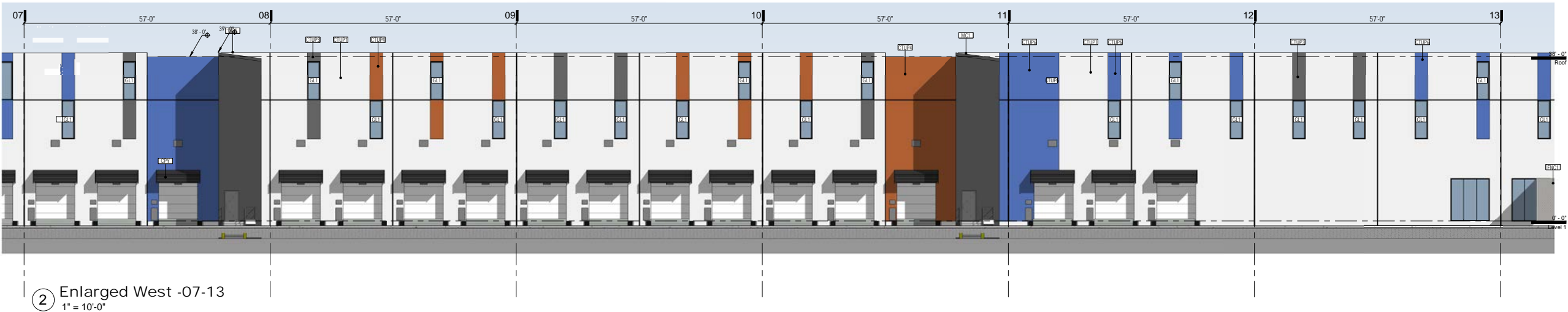
**GEBP - PH4 - Build to Suit**

**11208 Harris Road,  
Pitt Meadows**

### Enlarged East Elevation

PLOT DATE: 2021-05-03 8:44:06 AM

A304



KEYNOTE LEGEND	
TAG	DESCRIPTION
CPY	CANOPY
CTUP1	CONCRETE TILT-UP; PAINT FINISH, P1-LIGHT GREY
CTUP3	CONCRETE TILT-UP; PAINT FINISH, P3-CHARCOAL
CTUP4	CONCRETE - TILT UP - PAINTED P4 - ORANGE
CTUP6	CONCRETE - TILT UP - PAINTED P6 - BLUE
FNC1	FENCE TYPE 1: CHAIN LINK FENCING; C/W GATES AND HARDWARE, SEE SECTIONS & ELEVATIONS TO IDENTIFY HEIGHT, GALVANIZED.
GL1	VISION GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
GL2	SPANDREL GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
MC1	METAL CLADDING - CHARCOAL

42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION
4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

SCALE: As indicatedDATE:2021-05-03DRAWN: Author

PROJECT NUMBER18038

FILE PATH: \\P:\Projects\18038\18038.dwgPLOT DATE: 2021-05-03 8:44:15 AM

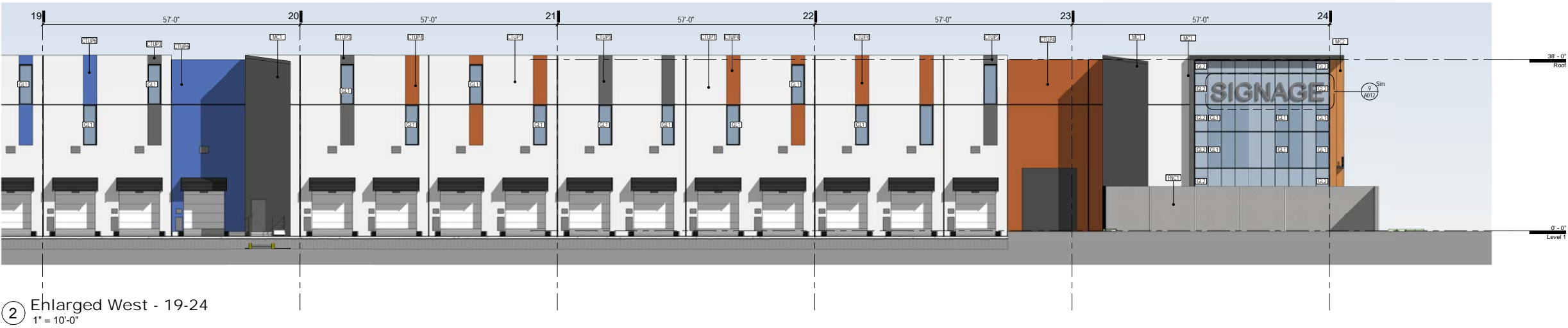
TKA+D  
ARCHITECTURE + DESIGN INC.  
200 - 1000 HARRIS ROAD, SUITE 100, PITT MEADOWS, BC V3Y 4P6

GEBP - PH4 - Build to Suit  
11208 Harris Road,  
Pitt Meadows

Enlarged West Elevation

PLOT DATE: 2021-05-03 8:44:15 AM

A305



KEYNOTE LEGEND	
TAG	DESCRIPTION
CTUP1	CONCRETE TILT-UP; PAINT FINISH, P1-LIGHT GREY
CTUP3	CONCRETE TILT-UP; PAINT FINISH, P3-CHARCOAL
CTUP4	CONCRETE - TILT UP - PAINTED P4 - ORANGE
CTUP6	CONCRETE - TILT UP - PAINTED P6 - BLUE
FNC1	FENCE TYPE 1: CHAIN LINK FENCING; C/W GATES AND HARDWARE, SEE SECTIONS & ELEVATIONS TO IDENTIFY HEIGHT, GALVANIZED.
GL1	VISION GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
GL2	SPANDREL GLAZING; SEE GLAZING TYPES & FINISHES SCHEDULE
MC1	METAL CLADDING - CHARCOAL
MC2	METAL CLADDING - FAUX WOOD - TYP. LONGBOARD

42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION
4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

SCALE: As indicated

DATE:

DRAWN: Author

PROJECT NUMBER

18038

FILE PATH:

PLOT DATE: 2021-05-03 8:44:23 AM

TKA+D

ARCHITECTURE + DESIGN INC.

300-1100 HARRIS STREET - VANCOUVER, BC, V6E 4P9

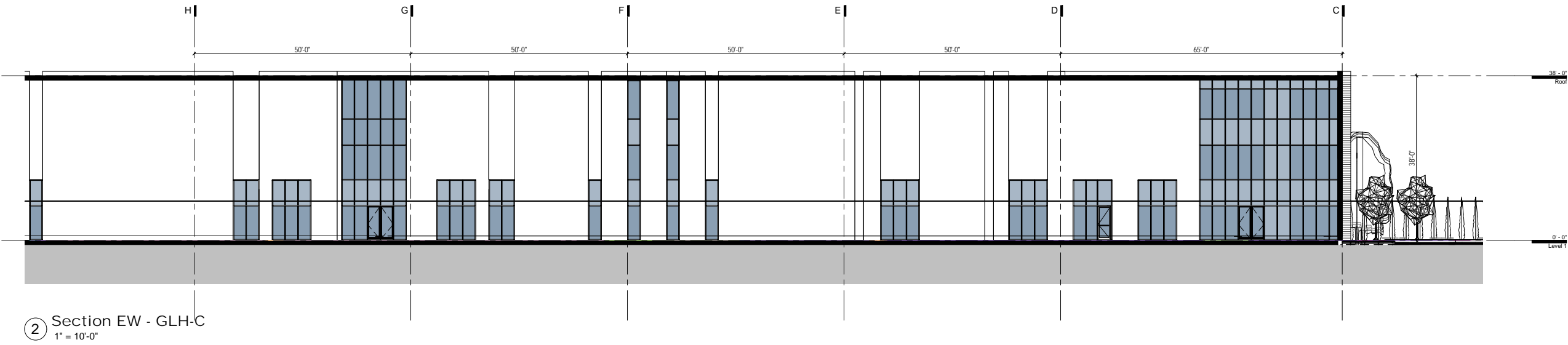
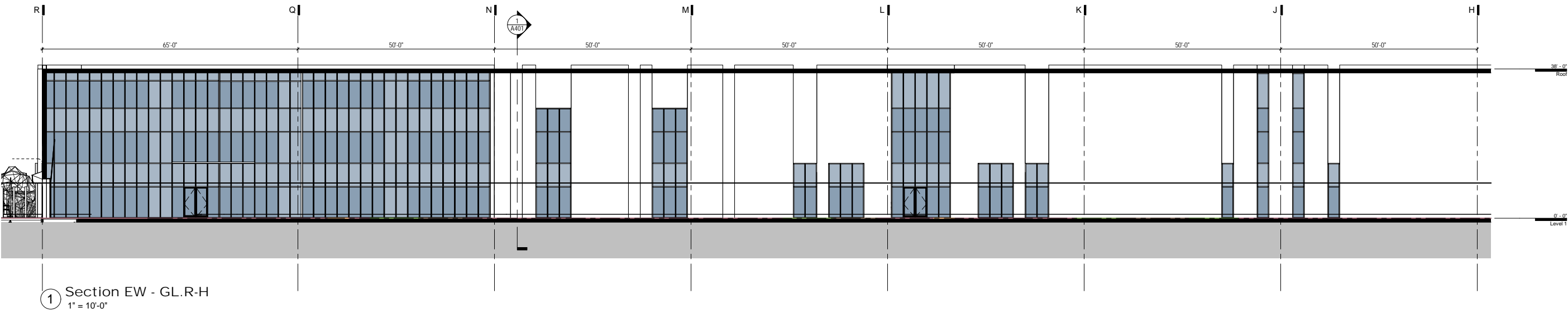
GEBP - PH4 - Build to Suit

11208 Harris Road, Pitt Meadows

Enlarged West Elevation

PLOT DATE: 2021-05-03 8:44:23 AM

A306



42021-05-03Re-issued for Development Permit

32021-03-05Re-issued for Development Permit

22020-05-08Re-issued for Development Permit

12020-03-10Issued for Development Permit

REV	DATE	DESCRIPTION
4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

SCALE: 1" = 10'-0"

DATE:

DRAWN: Author

PROJECT NUMBER

18038

FILE PATH:

PLOT DATE: 2021-05-03 8:44:31 AM

TKA+D

ARCHITECTURE + DESIGN, INC.

200 - 1000 HANCOCK STREET - WILLOWDALE, ONT. M2H 3P9

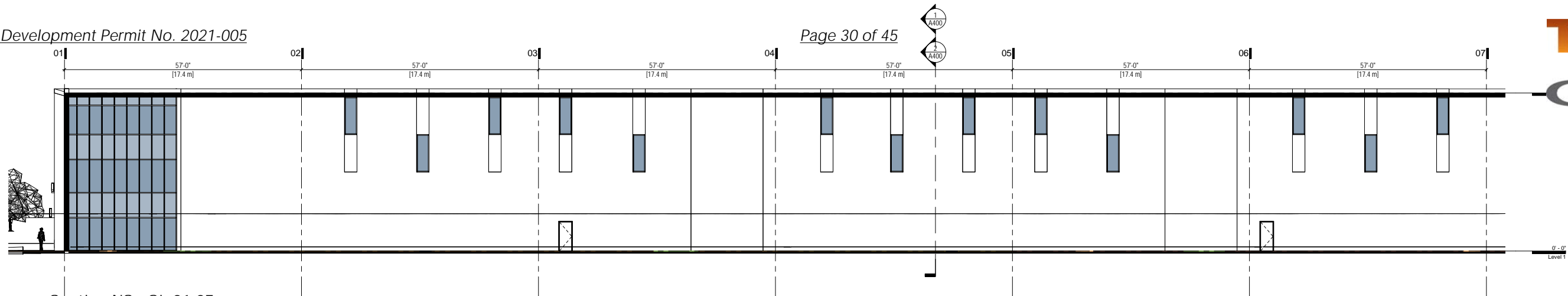
GEBP - PH4 - Build to Suit

11208 Harris Road,  
Pitt Meadows

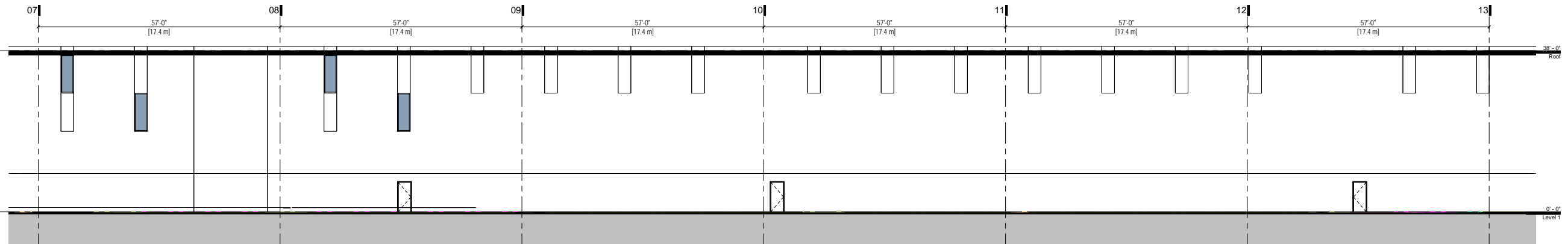
Building Section

PLOT DATE: 2021-05-03 8:44:31 AM

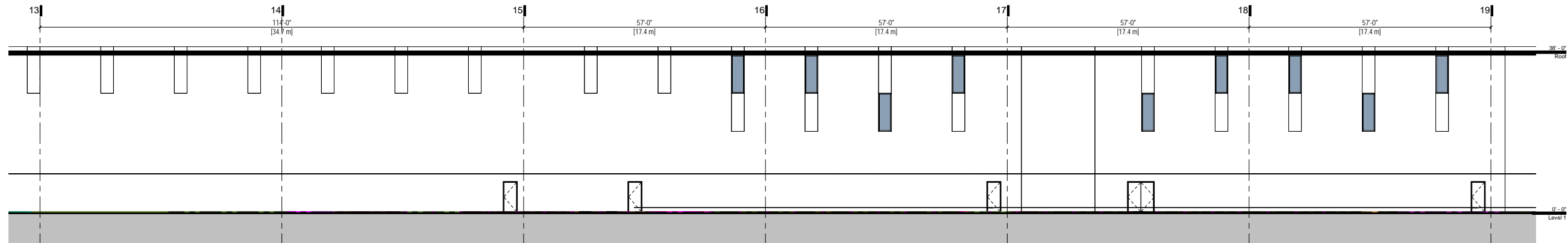
A400



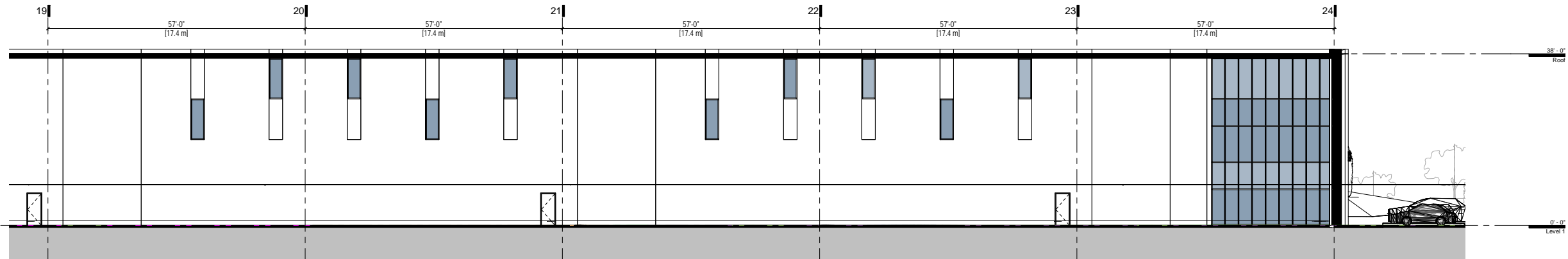
① Section NS - GL.01-07  
1" = 10'-0"



② Section NS - GL.07-13  
1" = 10'-0"



③ Section NS - GL.13-19  
1" = 10'-0"



④ Section NS - 19-24  
1" = 10'-0"

4 2021-05-03 Re-issued for Development Permit  
3 2021-03-05 Re-issued for Development Permit  
2 2020-05-08 Re-issued for Development Permit  
1 2020-03-10 Issued for Development Permit

REV	DATE	DESCRIPTION
4	2021-05-03	Re-issued for Development Permit
3	2021-03-05	Re-issued for Development Permit
2	2020-05-08	Re-issued for Development Permit
1	2020-03-10	Issued for Development Permit

SCALE: 1" = 10'-0" DATE: DRAWN: Author  
PROJECT NUMBER 18038  
FILE PATH: PLOT DATE: 2021-05-03 8:44:36 AM

TKA+D  
ARCHITECTURE + DESIGN, INC.  
200 - 1000 HARRIS STREET - VANCOUVER, BC, V6P 6P9

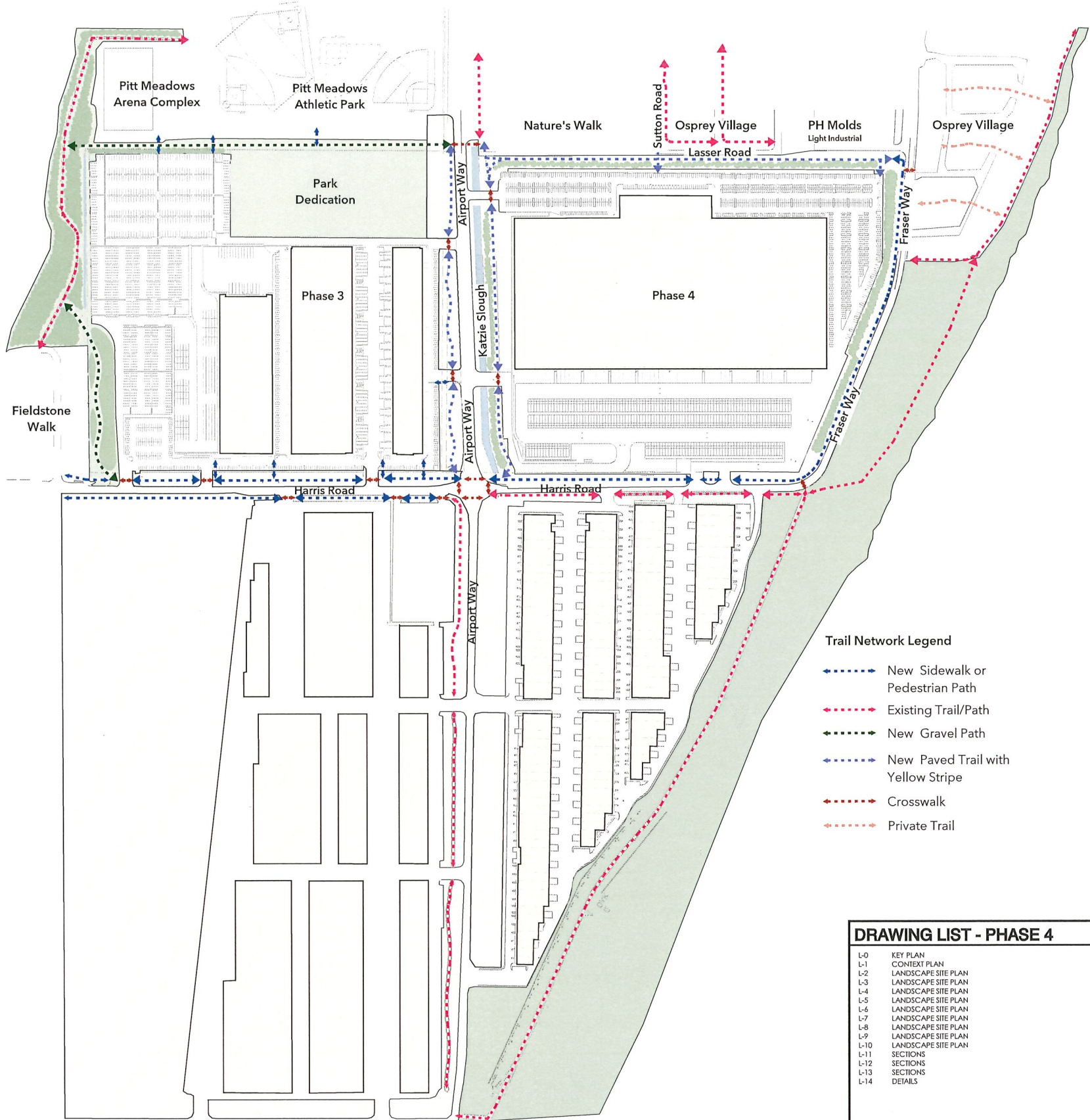
GEBP - PH4 - Build to Suit  
11208 Harris Road,  
Pitt Meadows

Building Section

PLOT DATE: 2021-05-03 8:44:36 AM

A401






- Trail Network Legend**
- New Sidewalk or Pedestrian Path
  - Existing Trail/Path
  - New Gravel Path
  - New Paved Trail with Yellow Stripe
  - Crosswalk
  - Private Trail

DRAWING LIST - PHASE 4

- L-0 KEY PLAN
- L-1 CONTEXT PLAN
- L-2 LANDSCAPE SITE PLAN
- L-3 LANDSCAPE SITE PLAN
- L-4 LANDSCAPE SITE PLAN
- L-5 LANDSCAPE SITE PLAN
- L-6 LANDSCAPE SITE PLAN
- L-7 LANDSCAPE SITE PLAN
- L-8 LANDSCAPE SITE PLAN
- L-9 LANDSCAPE SITE PLAN
- L-10 LANDSCAPE SITE PLAN
- L-11 SECTIONS
- L-12 SECTIONS
- L-13 SECTIONS
- L-14 DETAILS

Material Legend			
Detail	Key	Material	Description
		Hydroseed	Wildflower & Hard Fescue Mix by Premier Pacific Seed or approved equivalent
		Hydroseed	Shade Tolerant Wildflower & Grass Mix by Premier Pacific or approved equivalent
		Sod Lawn (Offsite)	
		Shrubs / Planting beds	Native Grass Seed Mix by Premier Pacific Seed or approved equivalent
		Structural Soil	To provide minimum 10 cubic meters per tree of growing medium
		Asphalt Path (with or Without Yellow Stripe)	
		Bird Houses	Make and Model TBD
		Landscape Boulders	Varies
		Picnic Table	Make and Model TBD
		Bench - with back	Make and Model TBD
		Bollard - Maglin 500 Series	Black Powder Coat
		Pole Lighting	To be coordinated with Electrical
		Street Lighting	
		Signalized Traffic Lights	Detail by DMD & Associates

Sym	Qty	Botanical Name	Common Name	Size/Space
Trees				
	151	Acer rubrum 'Bowhall'	Bowhall red maple	7cm cal. (col)
	18	Acer rubrum 'Sun Valley'	Sun Valley red maple	7cm cal. B+B
	31	Acer macrophyllum	Big Leaf maple	8cm cal. WB
	167	Quercus palustris 'Green Pillar'	Green Pillar pin oak	7cm cal. B+B
	40	Populus trembling 'Prairie Gold'	Prairie Gold trembling aspen	5cm cal. WB
	115	Pseudotsuga menziesii	Douglas fir	3m ht WB
	32	Picea sitchensis	Sitka spruce	2m ht B+B
	40	Tsuga heterophylla	Western hemlock	2.5m ht WB

Sym /Qty	Botanical Name	Common Name	Size/Space/Comments	
Shrubs				
	6780	*Arctostaphylos-uva-ursi	Kinnikinnick, bearberry	4" pot/1'-0" o.c.
	2562	*Cornus sericea "Kelsey"	Dward red-osier dogwood	#2 pot/30" o.c.
	1420	*Rosa nutkana	Nootka rose	#2 pot/3'-0" o.c.
⊗	1420	*Vaccinium ovatum	Evergreen huckleberry	#2 pot/3'-0" o.c.
⊗	1054	Calluna vulgaris	scotch heather	#2 pot/3'-0" o.c.
⊗	623	Euonymus fortunei "Emerald"	Fortune's spindle	#2 pot/36" o.c.
⊗	59	Hebe pinguifolia	Sutherland Hebe	#2 pot/30" o.c.
⊗	159	Lavandula angustifolia	Lavender	#2 pot/30" o.c.
⊗	704	Lonicera pileata	Box-leaved honeysuckle	#2 pot/2'-0" o.c.
⊗	466	Mahonia repens	Creeping Oregon grape	#2 pot/36" o.c.
⊗	345	Potentilla fruticosa	Shrubby Cinqufoil	#2 pot/36" o.c.
⊗	48	Rosa rugosa	Beach Rose	#3 pot/4'-0" o.c.
⊗	59	Stipa tenuissima	Mexican Feather Grass	#2 pot/3'-0" o.c.
⊗	88	Spiraea betulula	Birch Leaf Spirea	#2 pot/30" o.c.
⊗	707	Viburnum davidii	David's viburnum	#2pot/2'-0" o.c.
⊗	313	Taxus x media "Hicksii"	Hicks yew (male only)	1.5m ht/B+B/RB-RB

Asterix (\*) Plant in groupings of 35 - 50 (Plant arctostaphylos only at edge - consultant to determine on site)

Asterix (\*) Plant in groupings of 35 - 50 (Plant arctostaphylos only at edge - consultant to determine on site)

General Notes

- Planting Notes**
- All plants / planting to be per BCNTA and BCCLA standards.
  - Plant selection subject to availability at the time of planting.
  - Contractor shall source specified plant material and only offer area of search has been exhausted will substitutions be considered.
  - All trees to be staked in accordance with BCNTA Standards.
  - All plants to be sourced from nurseries certified free of F. ramorum.
  - Plant sizes and related container classes are specified according to the B.C. Landscape Standards current edition. For container classes #3 and smaller, plant sizes shall be as shown in the plant list and the standard; for all other plants, both plant size and container calls shall be as shown in the plant list. Specifically, when the plant list calls for #5 class containers, these shall be as defined in the BCNTA (ANSI) Standard.
  - All offsite trees to have minimum 15 cubic meters of growing medium unless otherwise specified.
- Soil Preparation and Placement Notes**
- All growing medium placed on project to meet or exceed BCNTA and B.C. Landscape Standards latest edition.
  - Submit sieve analysis by an approved independent soil testing laboratory for each type of growing medium being used on the project PRIOR to placement for review and approval. Clearly identify source and type for each. Resubmit as required until growing medium is approved. Provide one composite sample of each type of proposed growing medium for each different application within the project. Minimum 1 litre physical sample.
  - Submittals shall be made at least seven (7) days prior placement.
  - Contractor shall not move or work growing medium or additives when they are excessively wet, extremely dry, or frozen or in any manner which will adversely affect growing medium structure. Growing medium whose structure has been destroyed by handling under these conditions will be rejected. Growing medium shall not be handled in wet or frozen conditions.
- Irrigation Notes (Design-Build System)**
- Irrigation to be provided for all 'Soft Landscape Areas' (unless otherwise specified) shown on the drawing.
  - All 'Soft Landscape Areas' are to be irrigated with high efficiency design build irrigation system to IABC Standards. c/w rain sensor unless otherwise specified.
  - All irrigation valve boxes equipped with quick-couplers.
  - Layout of the system and installation shall be to trade standards for projects of this scale and type, to provide for uniform complete "head to head" coverage of all lawn and planted areas within the area as designated on the drawings.
  - Record Drawings: Submit with the operating and maintenance manual, a reproducible copy of the as-built condition of the system.
  - Instruct a designated representative of the Owner in the complete operating and maintenance procedures for that system, including winterizing the system for the first time with the designated representative observing.
- NOTE: Sleeving @ roadways to use schedule 40 cast iron sleeving.
- Quality Assurance**
- The irrigation system design and installation shall be in accordance with the Irrigation Industry of BC Standards and Guidelines.
  - All irrigation work shall be done by an experienced and competent irrigation contractor having the facilities and personnel adequate for the work specified. Minimum standard to IABC.

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Phase 4  
Pitt Meadows, BC**

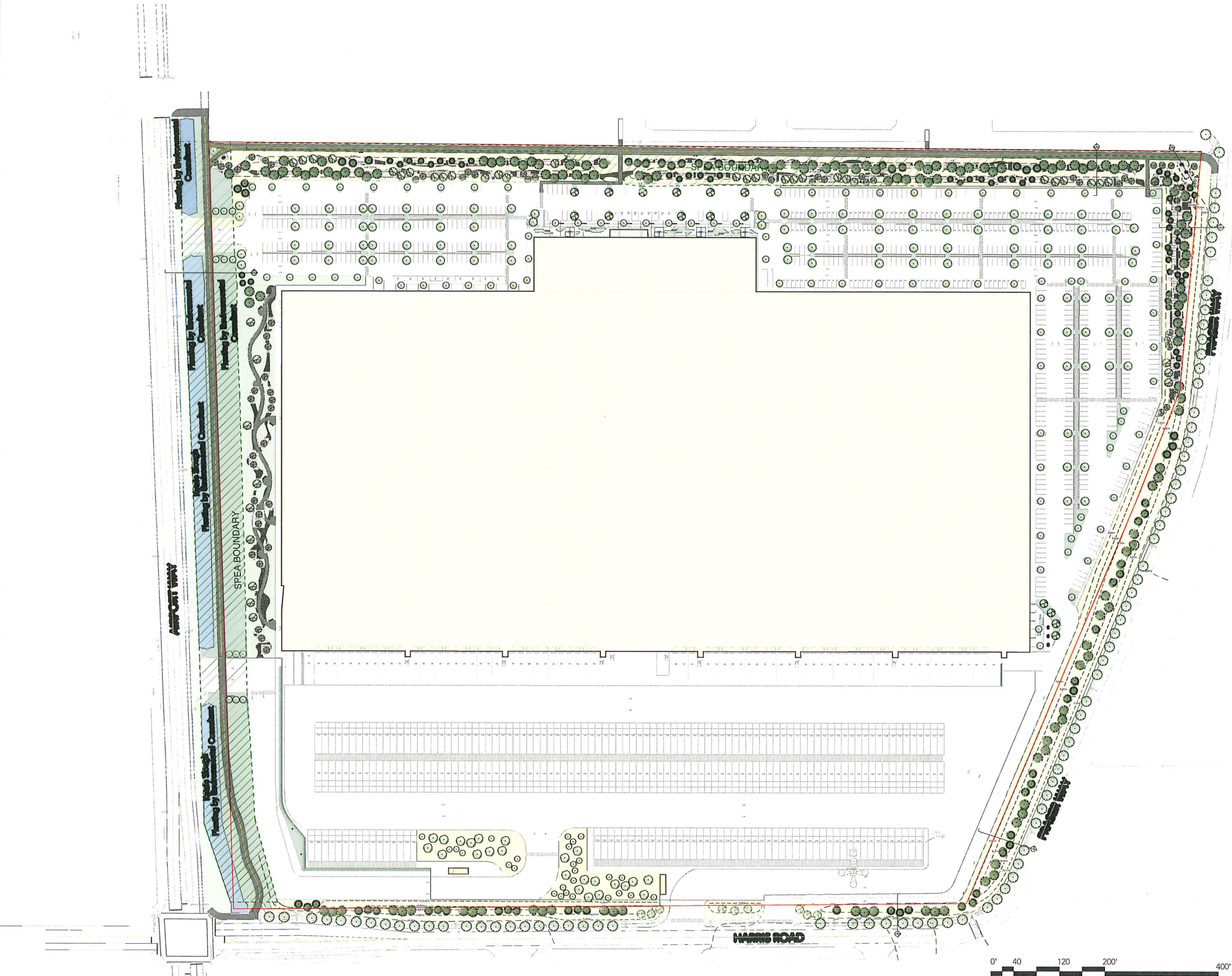
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**SRW PLAN**

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Sheet No.:

L-1



— Maglin Bollard 500

# Planting by Environmental Consultant

# Planting by Environmental Consultant

# Planting by Environmental Consultant

— Bench

— 2m Wide Gravel Path

- Shade Tolerant
- Hydroseeded Grass

### — Groups of Plants

— 4m Wide Asphalt Multi Use Path

— Dashed line denotes berm contours

F-4

**F-3**

6.00m.

7.00

+HP 7.75m

◆

\_\_\_\_\_

— Dogwood, Wild Rose,  
and Evergreen  
Huckleberry Bushes

— Structural Soil:  
92" x 77" x 30"

- Structural Soil:  
89" x 52" x 30"

— Structural Soil:  
111" x 115" x 30"

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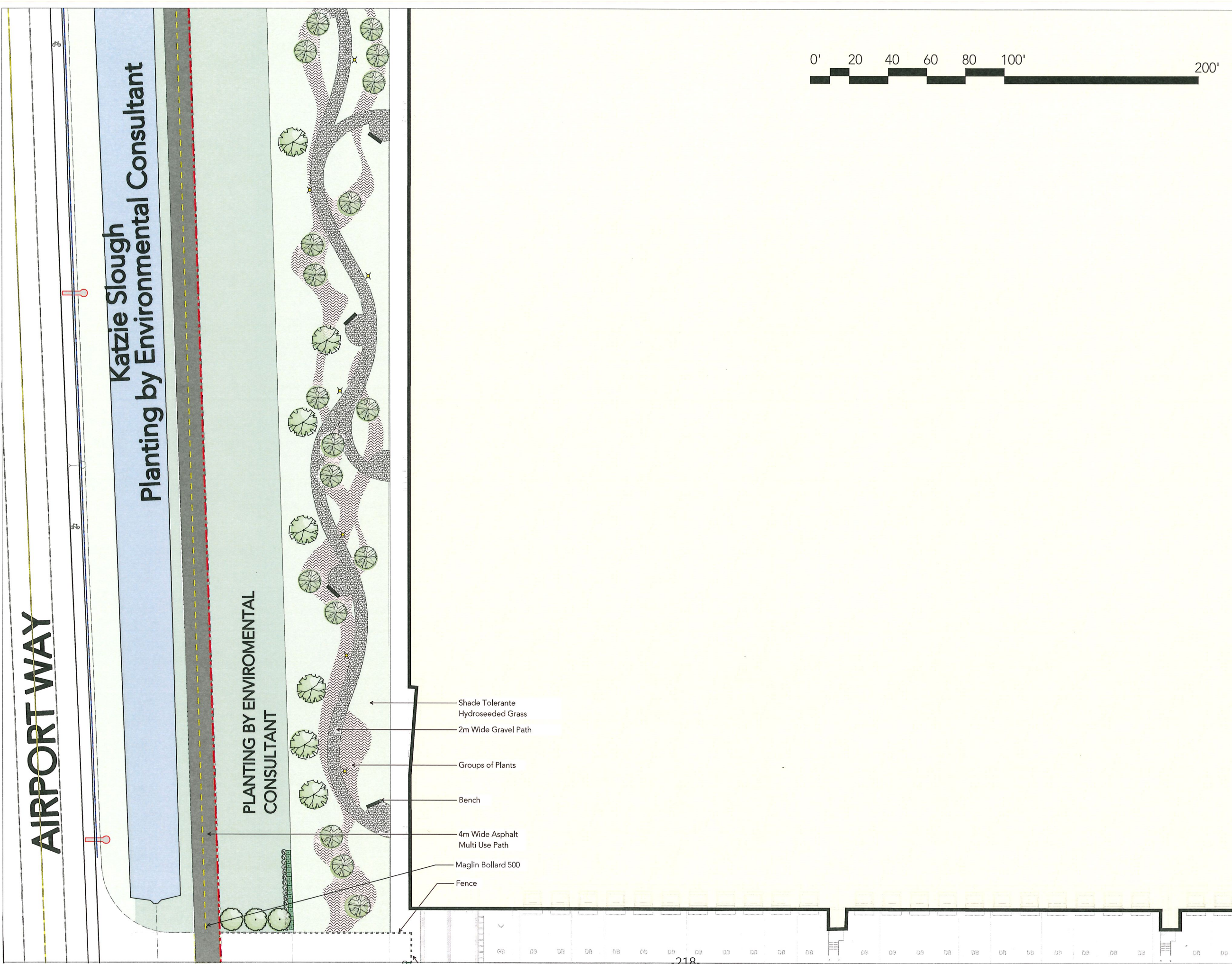
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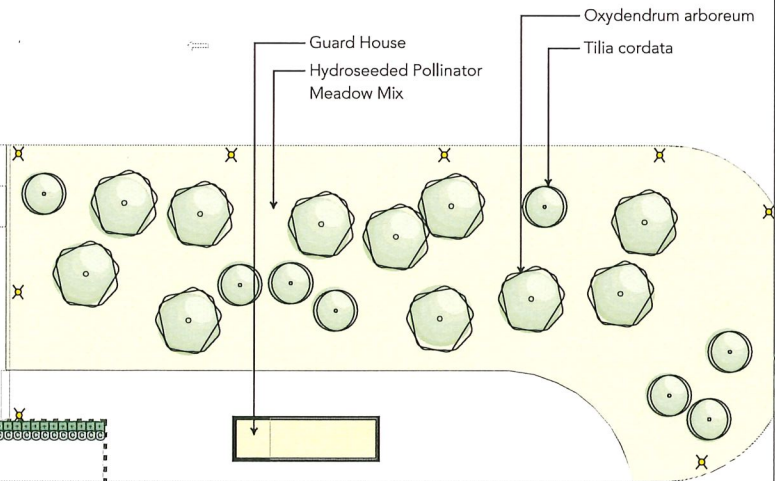
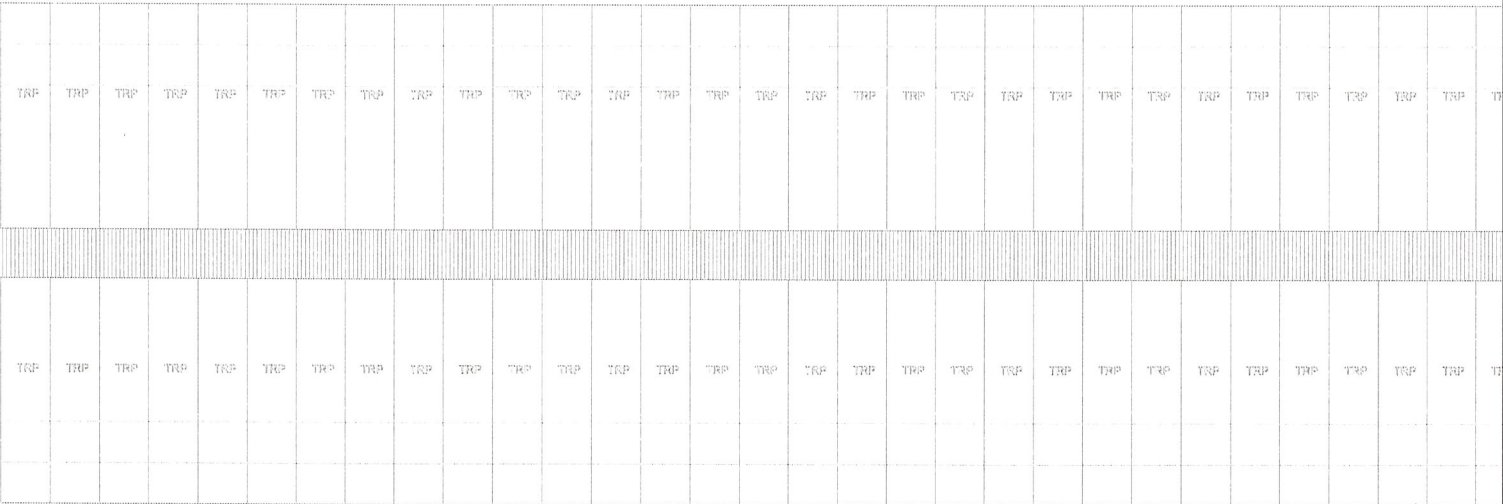


Katzie Slough  
Planting by Environmental Consultant

Raised Crosswalk  
Maglin Bollard 500  
Monument Signage by Arch.

Hedge to provide additional screening

Bike Racks  
Fence  
Pavers



4m Wide Asphalt Multi Use Path with Yellow stripe  
Maglin Bollard 500  
Monument Signage by Arch.

Street Light by DMD

Fence

Native mix - hydroseed

+5.00m  
+6.00m  
+6.00m  
+5.00m  
+4.00m

Sod lawn

Hydroseeded Pollinator Meadow Mix  
CIP Concrete sidewalk to City Standards

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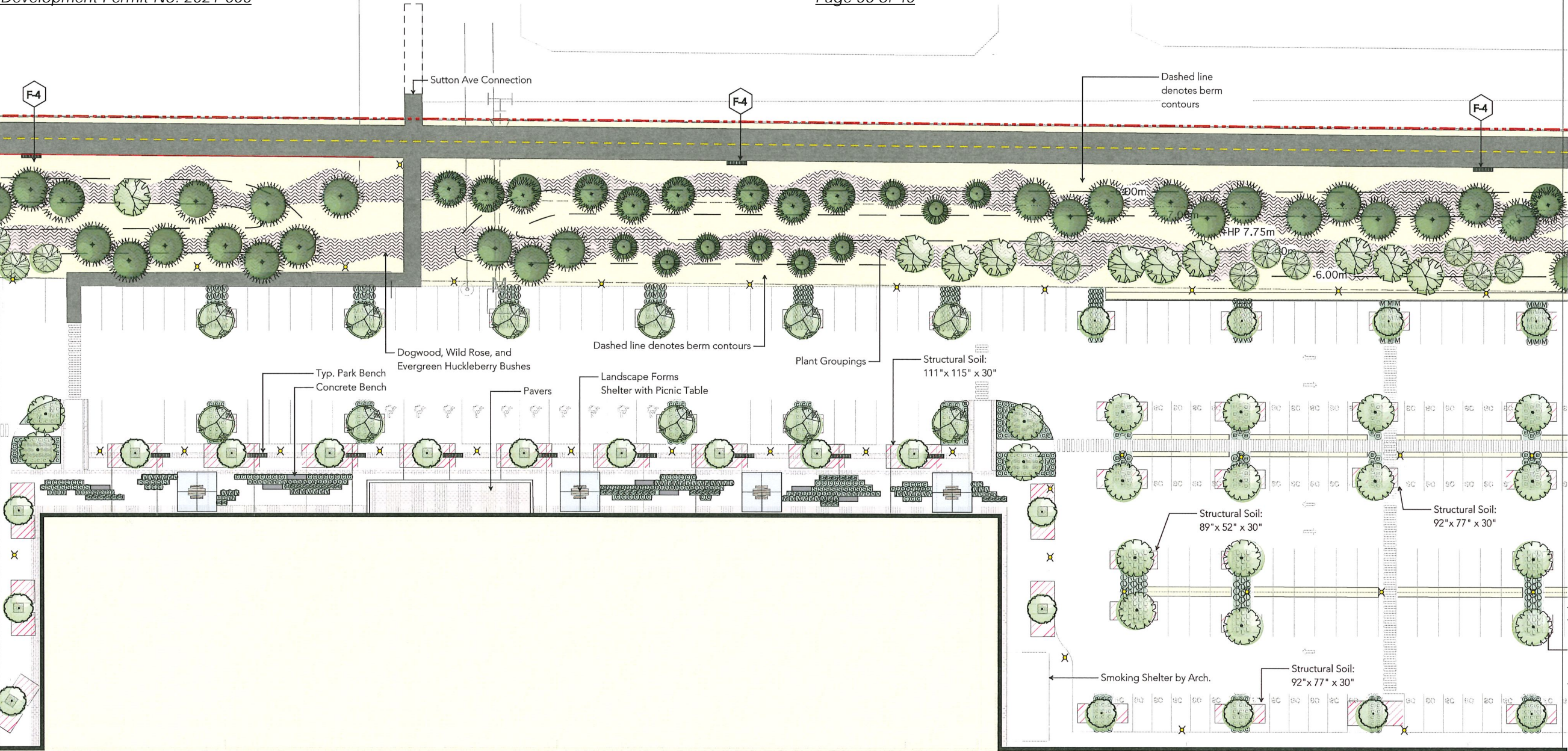
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Checked by: PK  
Date: Apr 30, 2019  
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**LANDSCAPE SITE PLAN**

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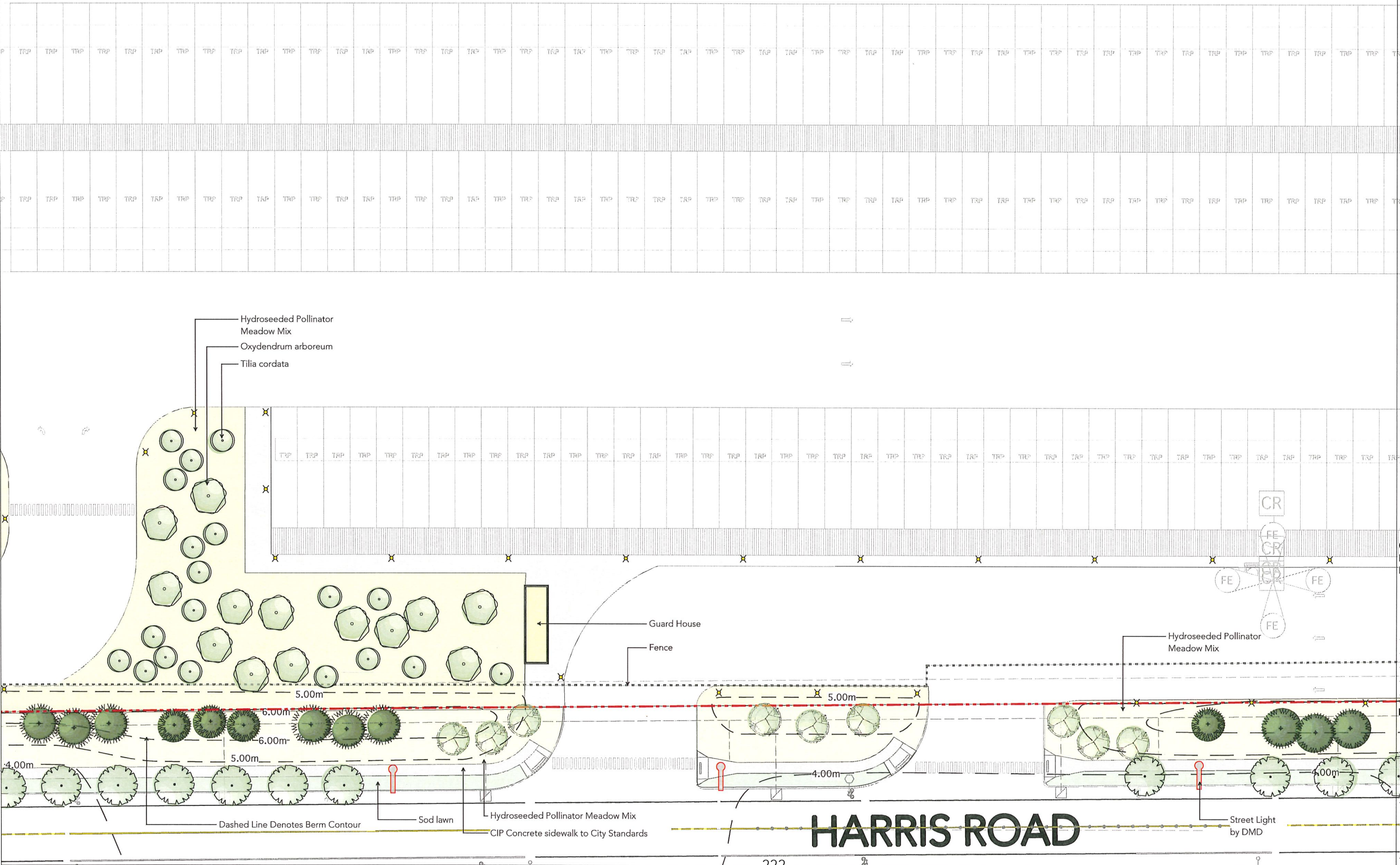
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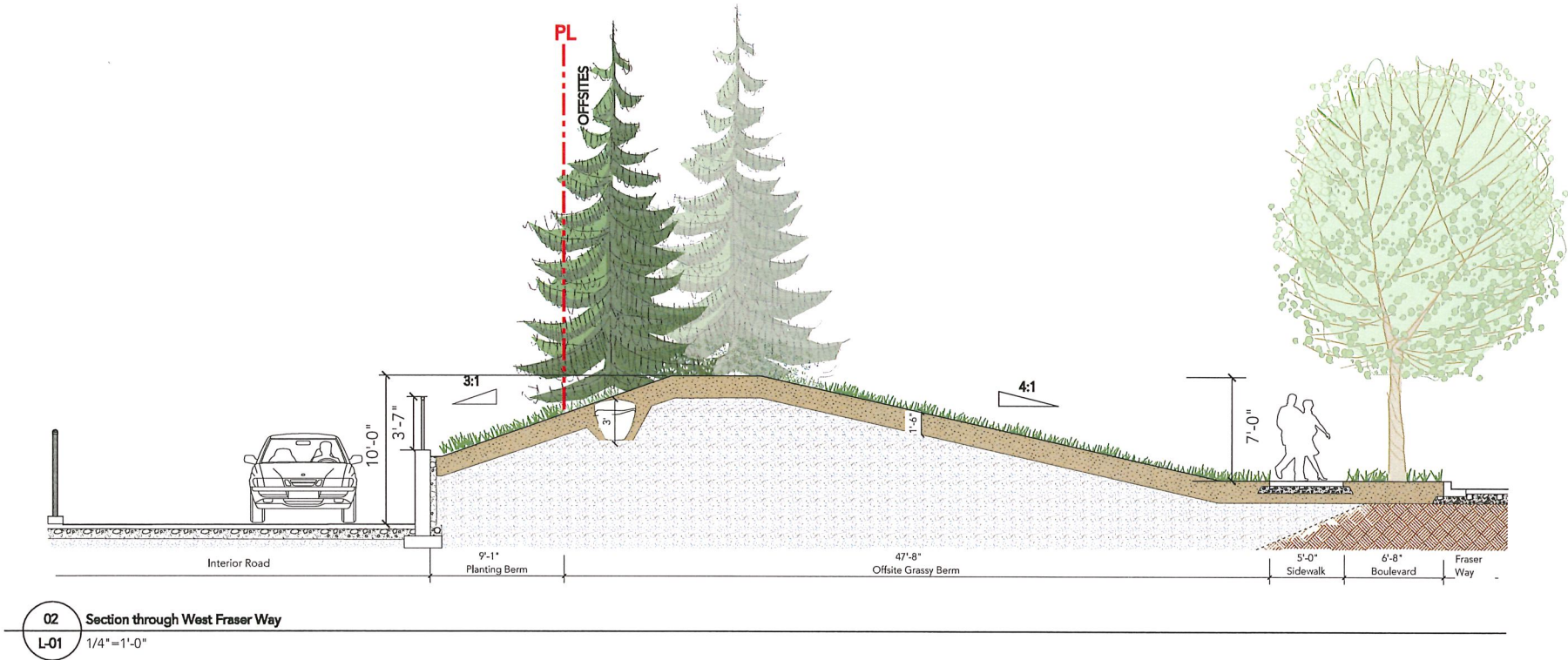
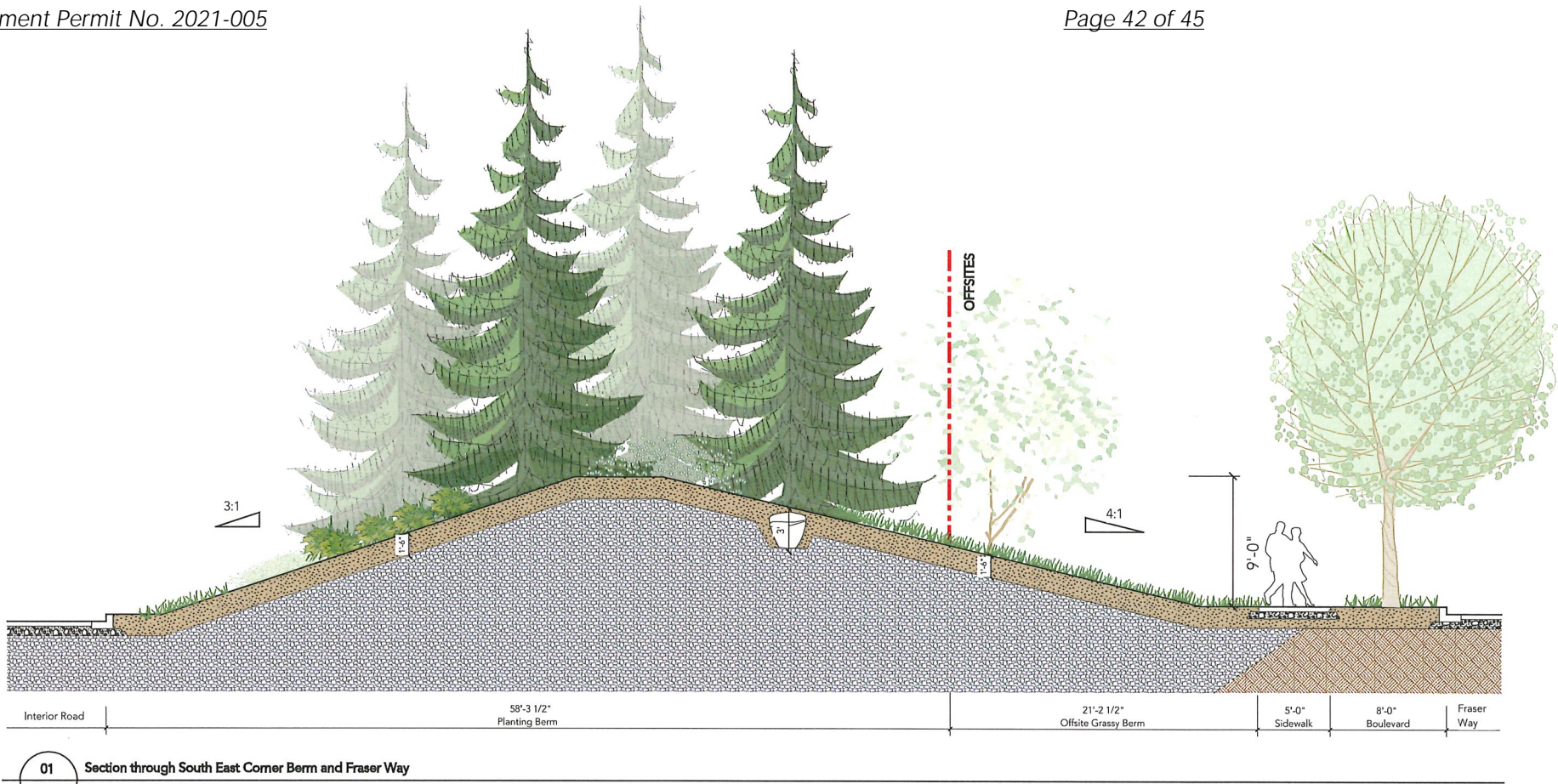
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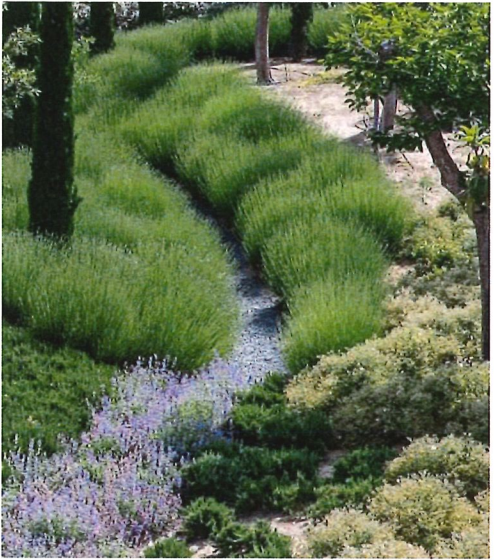








P1 Precedent Image: Picnic Areas with Custom Heavy Timbre Picnic Tables



P2 Precedent Image: Picnic Areas with Custom Heavy Timbre Picnic Tables



P3 Precedent Image: Meandering Path amongst planting and trees

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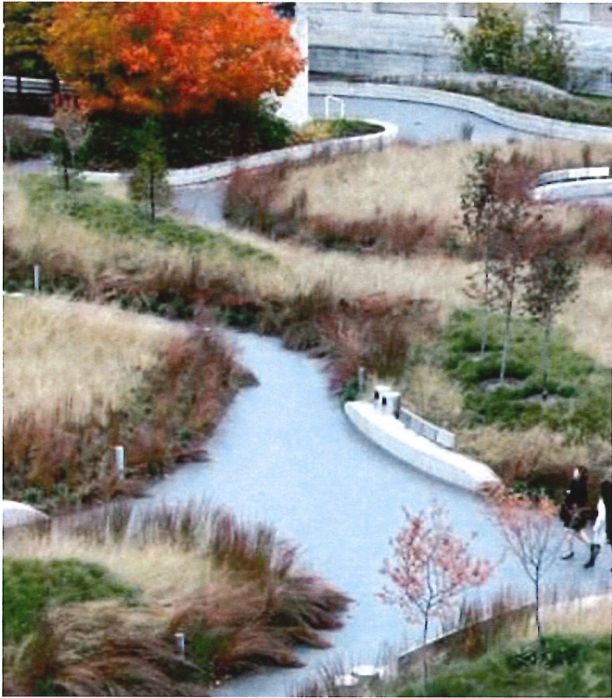
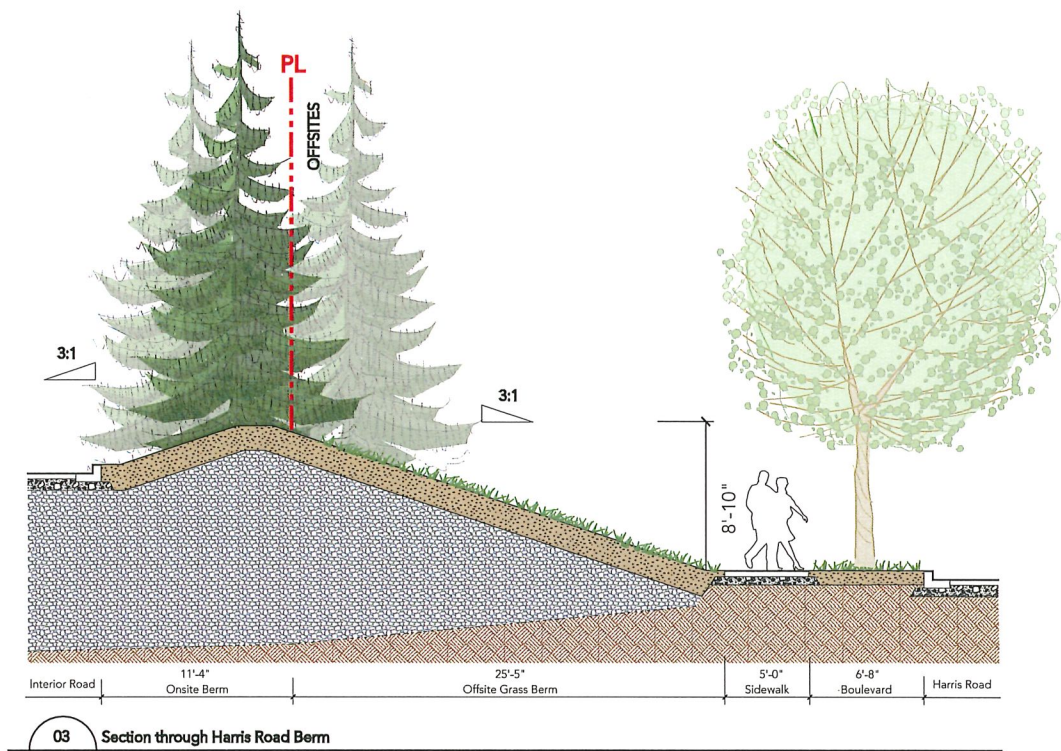
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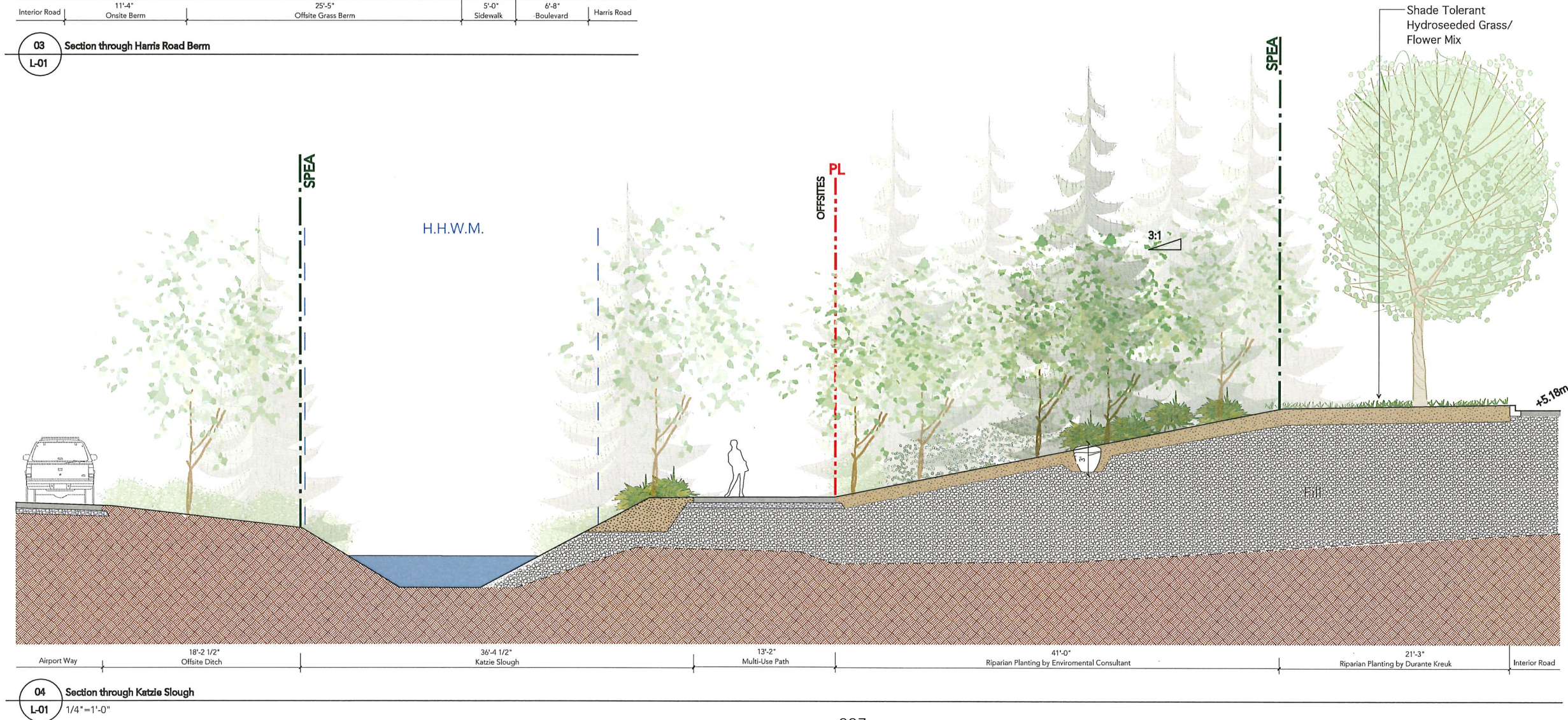
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**Landscape Sections**

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P4 Precedent Image: Meandering Path amongst native grasslands



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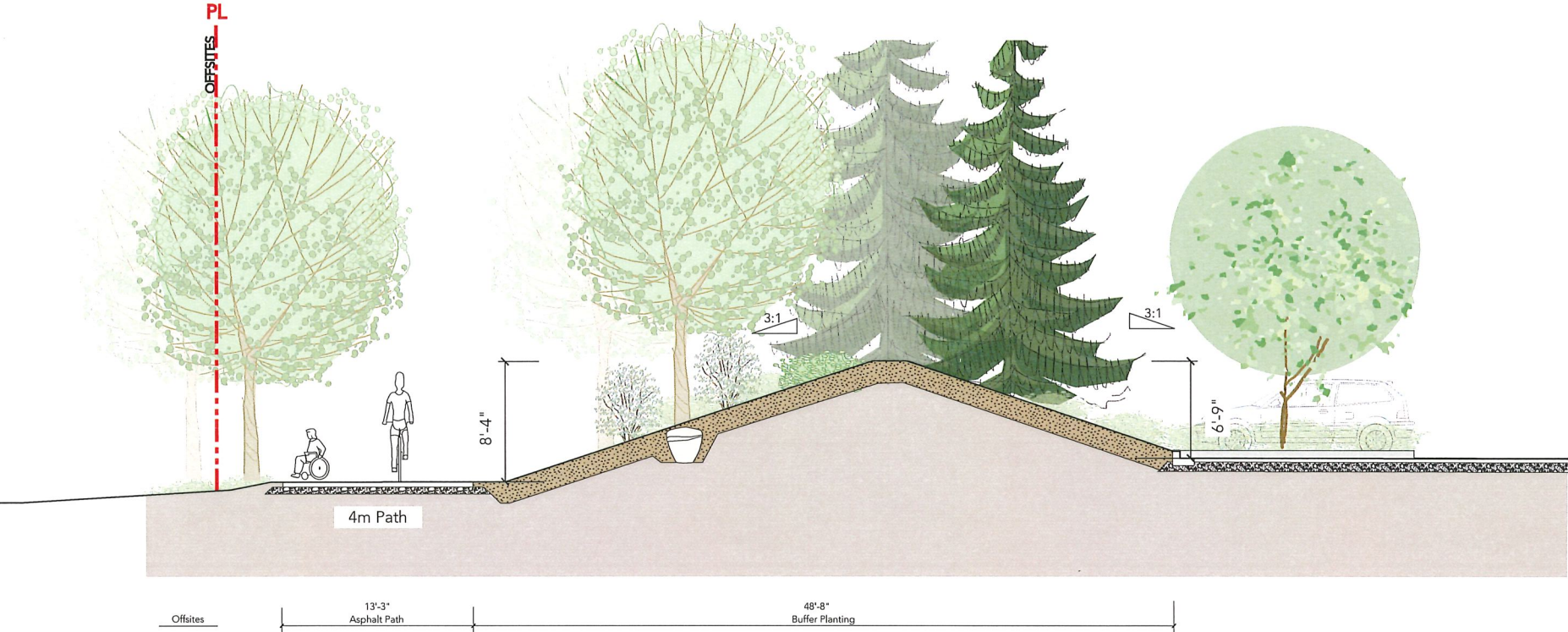
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**Landscape Sections**

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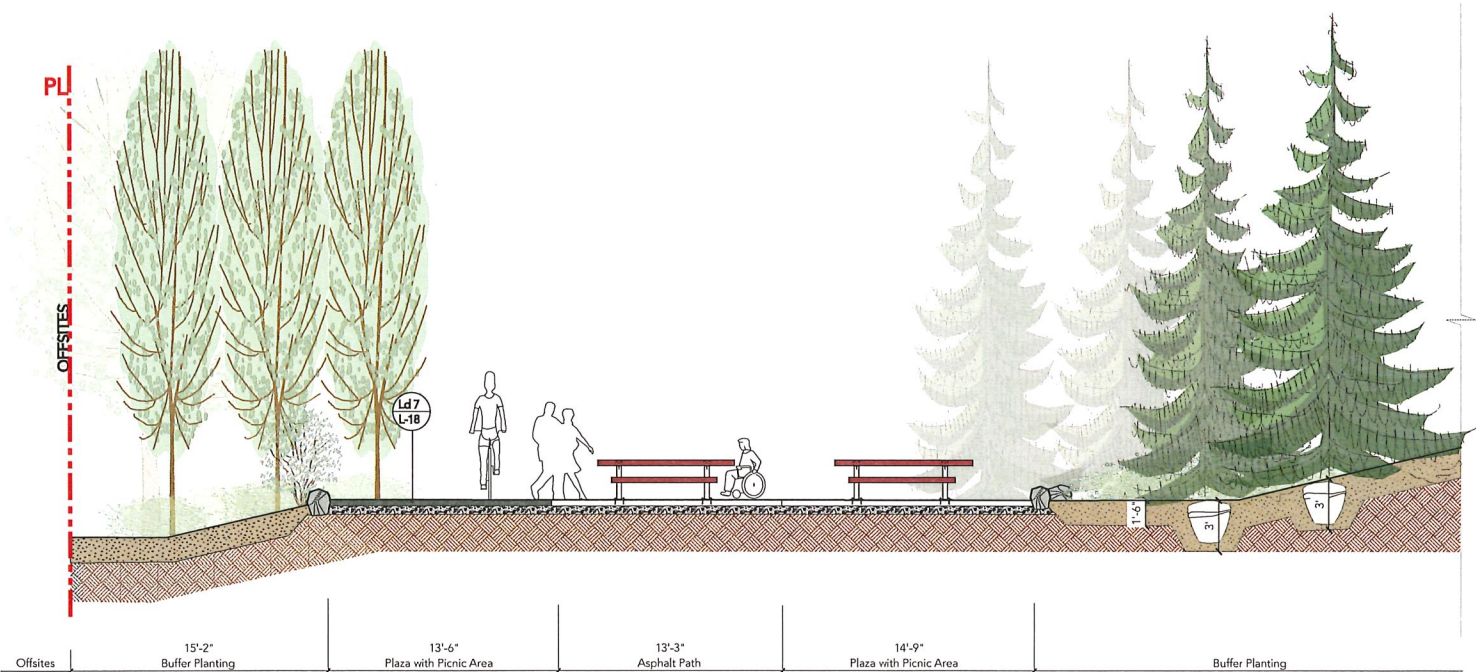
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05 Section through East Berm  
L-01 1/4" = 1'-0"



06 Section through South East Amenity Area and Berm  
L-01 1/4" = 1'-0"



P5 Precedent Image: Grove of Trembling Aspen



P6 Precedent Image: Planted Berm with Native Flowers and Tree Groupings

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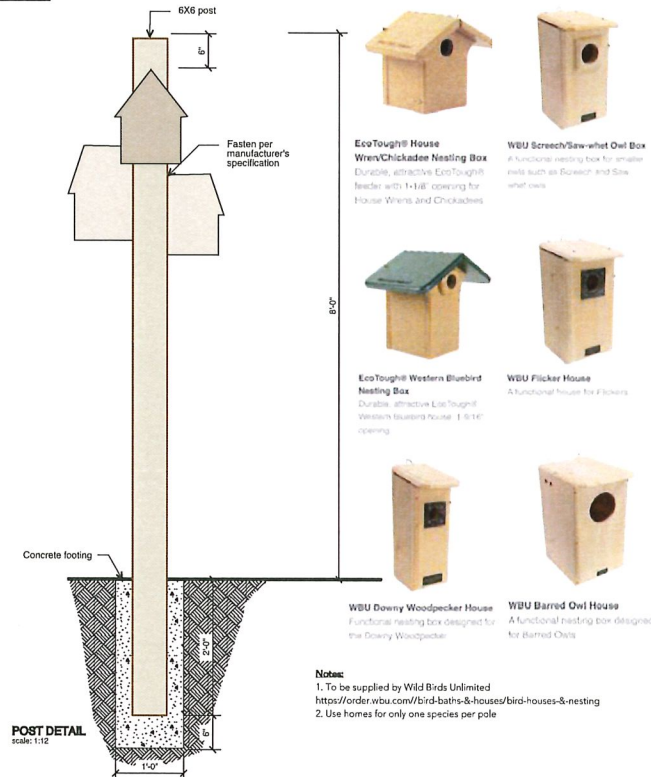
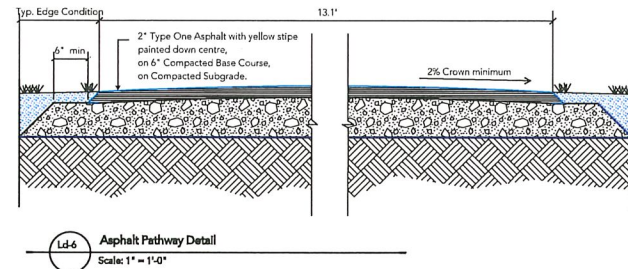
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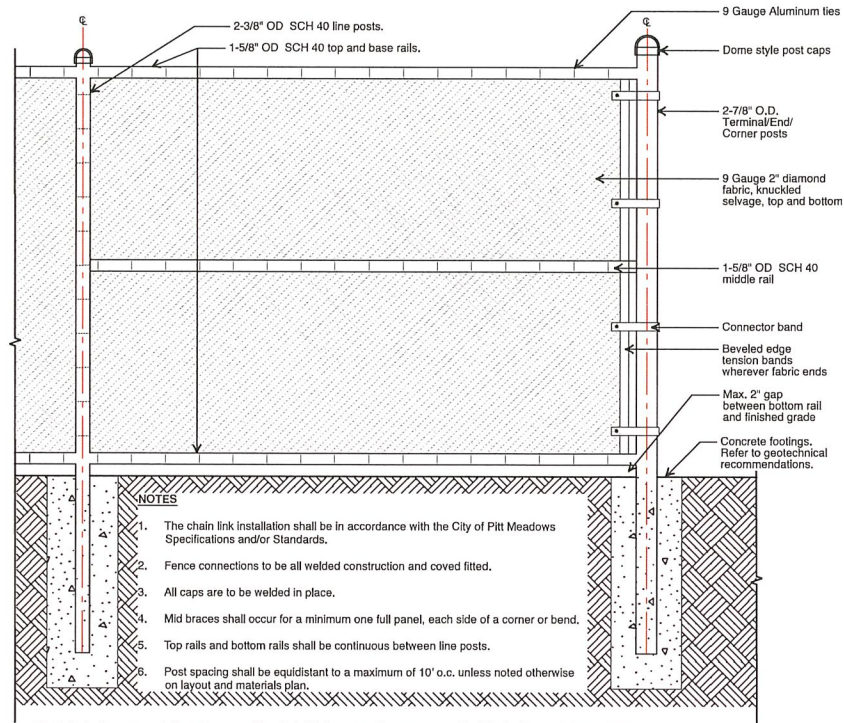
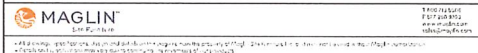
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Geotechnical recommendations: Fence post for chain link fences and gates up to 6.5' height shall be a minimum of 3' deep and 1" in diameter. The footings may be excavated using an auger drill or hydro-vacuum to allow for clean sides. A 6" thick layer of clean, free draining material shall be placed at the base of each footing.

Project:

**Golden Ears Business Park  
Phase 4, Pitt Meadows**

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