

Disclaimer: These minutes were prepared as a reasonable summary of the essential content of this meeting, not as a transcription.



Minutes of the Active Transportation Advisory Committee Meeting

April 11, 2022, 7:00 p.m.

Video Conference

Voting Members:	D. Bennett* H. Escaravage A. Evans P. Jongbloed D. Michalska G. Tejkel
Council Liaisons:	Councillor Miyashita*
Regrets:	J. Barnes R. Gaspar Councillor Hayes G. Kirkland G. Jay A. Ramji
Staff:	A. Gu, Project Manager J. Hart, Manager of Major Projects S. Maki, Director of Engineering & Operations A. Torres, Manager of Engineering & Facilities
Guests:	R. Armendariz- Rodriguez - – Vancouver Fraser Port Authority* M. Bekiaris – Vancouver Fraser Port Authority* D. Fitch – Vancouver Fraser Port Authority* B. Hall – Stantec* E. Langill — Vancouver Fraser Port Authority* R. Symons – ISL Engineering*
Secretary:	T. McCaw, Committee Clerk II

1. **CALL TO ORDER**

The meeting was called to order at 7:01 p.m.

2. **LATE ITEMS**

There was general consent amongst the Committee to add staff and member introductions to Item 6 of the Agenda.

3. **APPROVAL OF AGENDA**

It was **MOVED** and **SECONDED** THAT the Agenda for the April 11, 2022 Active Transportation Advisory Committee be approved as amended.

CARRIED

4. **ADOPTION OF MINUTES**

It was **MOVED** and **SECONDED** THAT the Minutes of the February 7, 2022 Active Transportation Advisory Committee meeting be approved.

CARRIED

5. **NEW BUSINESS**

5.1 Roles & Responsibilities of a Committee Member

T. McCaw, Committee Clerk II, provided a brief PowerPoint presentation of the duties and responsibilities of a City committee member, which forms part of the original minutes as **Attachment 1**.

Some of the highlights included:

- The review of expectations for members such as preparing for meetings, participating in voting and engaging with fellow members in an honest and respectful manner; and
- It was recommended that everyone reads and reviews the Committee Training manual and Training Videos, which can be found at:

<https://www.pittmeadows.ca/city-hall/committees/committee-orientation-and-training-resources>.

5.2 Active Transportation Network Review - Kick Off & Data Collection Locations

R. Symons, P. Eng. Sustainable Transportation Specialist with ISL Engineering, provided an overview of the Active Transportation Network Review, initial observations, key milestones and survey/engagement plans forming a part of the minutes as **Attachment 2**.

S. Maki, Director of Engineering and Operations, provided an update on the proposed pedestrian/cyclist data collection locations and sought the Committee's feedback. The following points were mentioned:

- Parks Department plans to obtain data along CPR Trail, Airport Trail, Parkside Trail and the Wildwood MUP;
- Previous spot counts were taken at Silver Bridge and South Alouette Bridge;
- Potential to put counters in a few of the locations listed below:
 - Baynes Rd Trail
 - Ford Rd, west of Baynes Rd
 - Park Rd
 - Hammond Rd
 - Bonson Rd
 - Dike trails near Airport or Kennedy Rd
 - Trans Canada Trail (south of Airport Way, east of Bonson Rd)
 - Hoffmann Park

The Committee participated in a discussion, with the following main themes noted:

- Questions were raised around the amount of time the survey would be open for and whether it would be incentivized in an effort to gather more feedback (*Staff noted that no timeline has been identified yet, but likely in a month for a few weeks time. It will also be supported through the City's Communication channels (website, social media, and digital board in front of City Hall). There are no plans to incentivize. Staff will distribute the survey to the Committee once released*);
- A question was raised around vehicle traffic volume and the safe incorporation of cyclists on a road (*R. Symons stated when there are more than four thousand vehicles a day and if vehicles are travelling over 50 km/hr, it is recommended that cyclists need to be physically separated from traffic by a safety barrier*);

- The question was raised on where maps for local pathways will be available (*Staff and R. Symons indicated that the maps will be a part of the survey, and a part of the Active Transportation Network Review, which will be digitally available online*);
- Discussions were held around whether ISL was going to be reviewing the 2012 Pedestrian and Cycling Master Plan in an effort to review what important items still had not been completed (*Staff stated there will be a review in addition to the tracking sheet on what has been completed and what is still to be done. It was stated that there are a number of improvements in progress such as trail improvements along Airport Way*); (*R. Symons stated that active transportation guidelines have changed since 2012, likely altering some of the past recommendations*); and
- The question was raised whether the Active Transportation Network Review would be completed in time to be incorporated into the changes planned for the Airport Way expansion (*Staff clarified that the timelines will not align and the scope of work tied to the Airport Way expansion will not be changed as they are tied to developer agreements that are currently in place*).

* R. Symons exited the meeting at 7:38 p.m.

5.3 Harris Road Underpass - Feedback on Active Transportation Design

R. Armendariz-Rodriguez, Senior Project Delivery Specialist from the Vancouver Fraser Port Authority (VFPA), presented and requested feedback from the Committee on active transportation aspects of the Harris Road Underpass, which forms part of the minutes as **Attachment 3**.

The Committee participated in a discussion, with the following main themes noted:

- Questions were raised around what type of surface would be used for the Multi-Use Path (MUP) and if there would be any coloring used to identify and support pedestrian and cycling movement and direction (*VFPA stated that a consistent surface material (asphalt) is intended to be used; City staff mentioned that pavement markings, potentially a centreline and signage could be used*);
- Clarification around whether the unidirectional paths were for pedestrians and cyclists (*VFPA stated that the unidirectional paths would only be for bicycle traffic*);

- The recommendation was made to have education and signage to help support compliance with the recommended movement patterns;
- Concerns were raised around how cyclists will access the West Coast Express from 124 Avenue without crossing Harris Road two times (*Staff stated that although there are no plans for a midgrade crossing as part of this project, the underpass structure would be built with consideration of a potential future mid-block structure so not to introduce new constraints should this be implemented in the future as a separate project*);
- The question was raised around how other similar areas are managing cyclists and pedestrians in similar fashions (*VFPA stated there are examples in Vancouver, one being the causeway that supports cyclists and pedestrians to and from the Lions Gate Bridge; however, in this location, there is a healthy division for cyclists, pedestrians and automobiles*);
- Support was given for the unidirectional recommendation for cyclists;
- The question was raised if it was recommended people use the Bonson Road overpass to access the West Coast Express since there are no plans for a mid-block crossing (*Staff mentioned that it is unlikely that route would be more convenient for users and that they would likely still use Park Rd to Harris Rd*);
- Concerns were raised around whether there was any engagement with the residents of Keystone building (12350 Harris Road) (*Staff and VFPA stated there have been stakeholder meetings held with the Keystone Strata detailing the current status, as well as, the future development details; feedback has been provided and considered*);
- Confirmation that the garbage and benches identified in the images and drawings will not impede on the allocated 7m MUP along Harris Road or the 6m MUP identified at the rail crossing (*VFPA confirmed it will not impede on the MUP as identified*);
- Discussions were held around potential signage along the MUP with the recommendation to keep consistent signage throughout the City (*Staff mentioned that there isn't consistent/template signage currently, but wayfinding is being reviewed as part of the Active Transportation Network Review and possibly outcomes of that can be incorporated into future signage plans*);
- Discussions were held around the type of materials recommended to be used for the MUP;

- Discussions were held around the recommended lighting including the adequate appropriate usage for the MUP;
- Questions were raised in an effort for a deeper understanding around the plans for proper drainage to avoid water pooling and potential flooding (*VFPA stated that the surfaces will be sloped to accommodate drainage, there will be a pump station to manage the water and designs will include the increased volumes and frequency observed in recent storm events and the impacts of climate change*);
- Concerns were raised around the design for the suggested unidirectional cycling supporting speeds of 35 km/hr (*VFPA stated this speed is in alignment with the BC Active Transportation guidelines which is widely adopted and used in design*);
- Discussions were held around the space allocation for pedestrians versus cyclists on the MUP and how it will transition if it is narrowed at the rail portion. There was recommendation to give more space to cyclists;
- Questions were raised for individuals using mobility aids and clarity on which side of the MUP to use; and
- Questions were raised around the plans for Harris Road during the construction of the underpass (*VFPA clarified that Harris Road will go down to three lanes, split of the directions will be outlined in the future Traffic Management Plan*).

**All guests from the Vancouver Fraser Port Authority & Stantec exited the meeting at 8:45 p.m.*

5.4 Update on ATAC Project Priorities List

S. Maki, Director of Engineering & Operations, provided a verbal update on all completed, in progress and deferred items detailed in the ATAC Project Priorities list forming a part of the minutes as **Attachment 4**.

- A recommendation was made to clean up the Lougheed Trail near Kennedy Rd as some areas are dirty and muddy and some overgrown vegetation, creating a safety concern. Staff to follow-up with the Parks Department.

** D. Bennett exited the meeting at 8:46 p.m.*

5.5 Update on the ATAC Action Item Tracker

S. Maki, Director of Engineering & Operations, provided a verbal update on completed action items detailed in the ATAC Action Item Tracker forming a part of the minutes as **Attachment 5**.

There were no further discussions or comments made by the Committee on this topic.

5.6 Annual Committee Presentation to the Engagement & Priorities Committee (EPC)

S. Maki, Director of Engineering & Operations, provided a brief synopsis of the details and expectations around the June 14, 2022 EPC.

Committee members participated in a discussion with the following main themes captured:

- What to include in the presentation to the EPC, the grants received and the work that was completed. *(Staff recommended identifying and discussing items such as current projects, cycling advocacy and annual active transportation communications campaign, HUB education and awareness, the Active Transportation Network Review, accessibility and wayfinding initiatives, local improvements such as various leading pedestrian intervals, future areas of focus and the Harris Road underpass project. Inclusion of photos of previously completed improvements at Silver Bridge and S. Alouette Bridge, Baynes/Ford Rd and Baynes/Airport Way intersections were also mentioned).*

**T. Miyashita exited the meeting at 8:59 p.m.*

5.7 Update on the ATAC Work Plan

S. Maki, Director of Engineering & Operations, provided a verbal update on the ATAC 2022 Work Plan forming a part of the minutes as **Attachment 6**.

There were no further discussions or comments made by the Committee on this topic.

5.8 Committee Meeting Format

The Committee members were given the opportunity to speak and vote on how they would like to see future Active Transportation Advisory Committee (ATAC) meetings held in the future, whether it be in person or through video conferencing.

The Committee was unable to participate in a formal vote on how they would like to see future ATAC meetings held as quorum was lost. Topic to be revisited at June ATAC meeting.

Committee members participated in a discussion with the following main themes captured:

- The recommendation was made to hold hybrid Committee meetings (*Staff clarified that the City does not have the technical capabilities to host hybrid committee meetings; however, a recommendation was made to consider holding in person meetings at City Hall at least once throughout the year*); and
- The recommendation was made that the Committee meet in person when reviewing the Active Transportation Network Review.

6. ROUNDTABLE

The Committee participated in a roundtable discussion and introduction of all staff and committee members.

The Committee did not provide any formal motions or recommendations; an open discussion was held with the following main themes noted:

- Interest of hosting another Bike Valet during the Pitt Meadows Day Parade and a call out for volunteers from the Committee. P. Jongbloed will follow-up with an email to the Committee;
- A suggestion was made around using this opportunity to provide a survey or questionnaire to residents to help gather information for the Active Transportation Network Review;
- A recommendation was made to collect donations at the Pitt Meadows Day Bike Valet station in an effort to support a local active transportation and accessibility charity; and

- Concerns were raised around student safety walking to school buses and to and from school.

7. SUMMARY OF ACTION ITEMS

1. Staff to follow-up with Parks Department on condition of Loughed Trail, specifically at west end near Kennedy Rd.

8. ADJOURNMENT

The meeting was adjourned at 9:17 p.m.

The next *Active Transportation Advisory Committee Meeting* is June 13, 2022 at 7:00 p.m.

Certified as correct:

Tatiana McCaw, Committee Clerk II

Active Transportation Advisory Committee

Welcome!



ATAC - Summary

- Longstanding committee;
- To provide advice and recommendations to Council and staff regarding issues related to active transportation and accessibility in the community; and
- Considers educational opportunities for promoting active transportation in the community.



Committee Members

Responsibilities:

- Prepare for and attend meetings
- Let Chair/Clerk know if you can't attend
- Vote on recommendations put forward by the Committee
- Engage in honest and respectful dialogue at all times
- Follow City policies



Training Materials

pittmeadows.ca/city-hall/committees

You'll find:

- TOR's
- Training manual
- Policies
- Training videos



Privacy Protection

We are each responsible for the protection of personal information.

Only use and disclose personal information for the reason for which it was provided.



Questions?





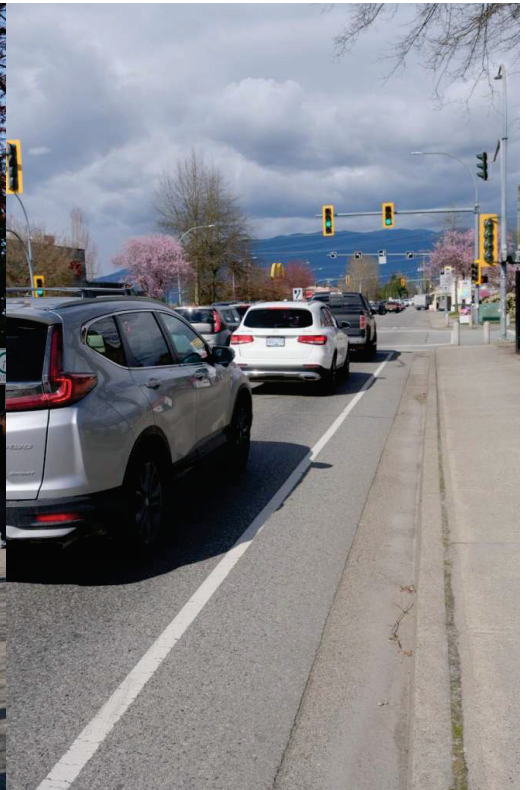
PITT MEADOWS

ACTIVE TRANSPORTATION PLAN INTRODUCTION AND NEXT STEPS

APRIL 11, 2022

ROY SYMONS P.ENG.
SUSTAINABLE TRANSPORTATION SPECIALIST

ISL



ISL

BIKE LANES





NEIGHBOURHOOD BIKEWAYS

PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 3



PATHWAYS

PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 4





RURAL ROADWAYS

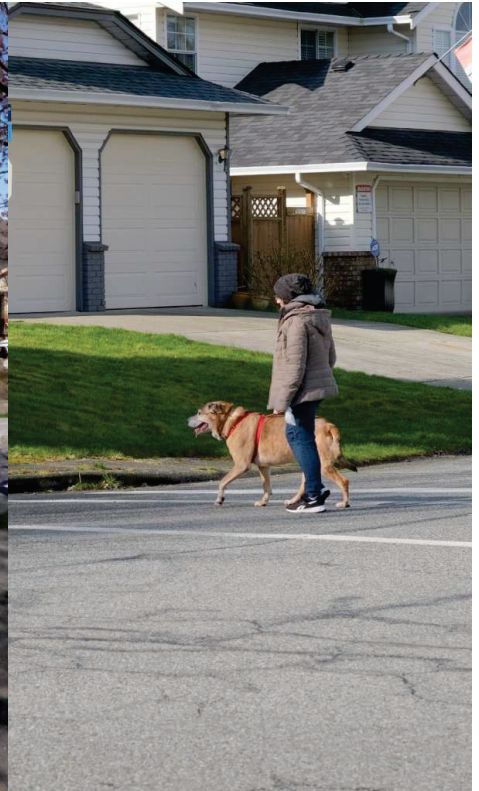
PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 5



DYKE TRAIL

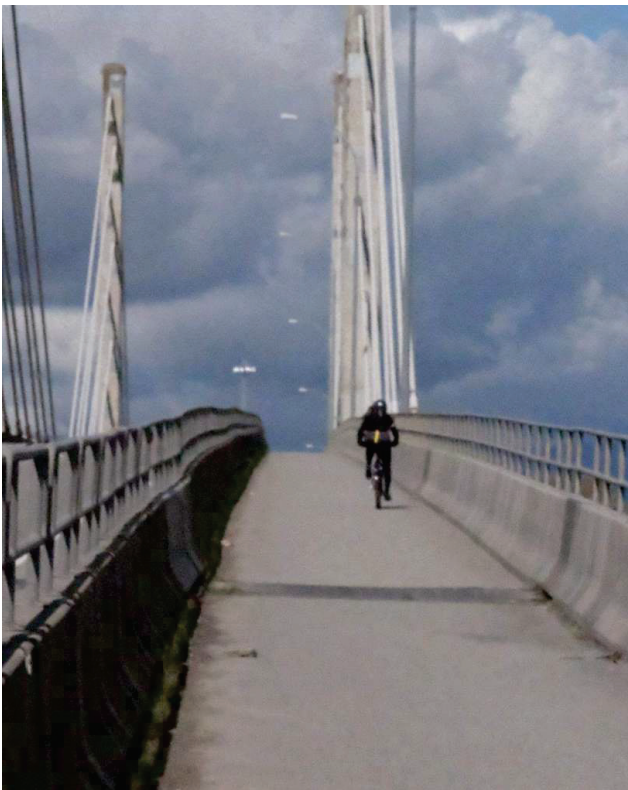
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ISL CROSSINGS

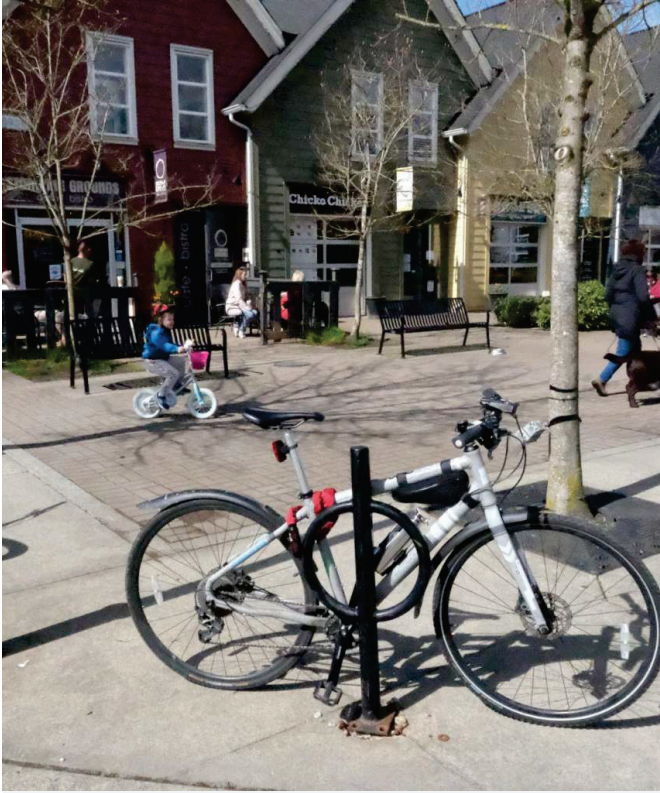
PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 7



ISL EXTERNAL CONNECTIONS

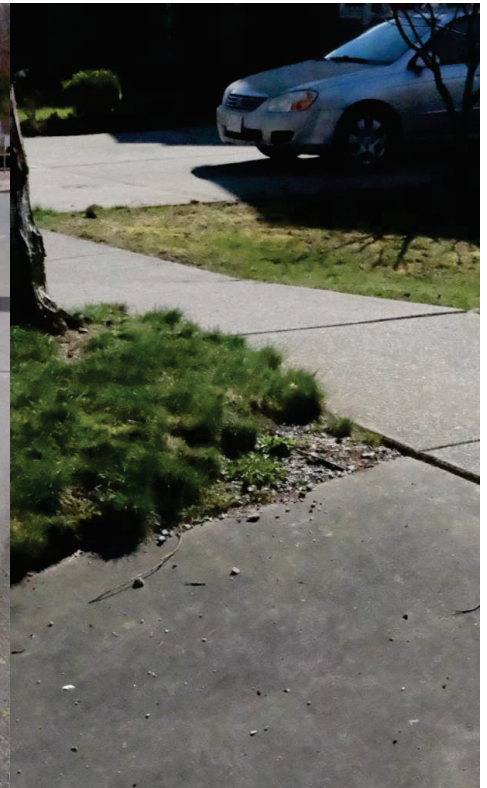
PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 8





AMENITIES (I.E., BIKE PARKING)

PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 9



MAINTENANCE ISSUES

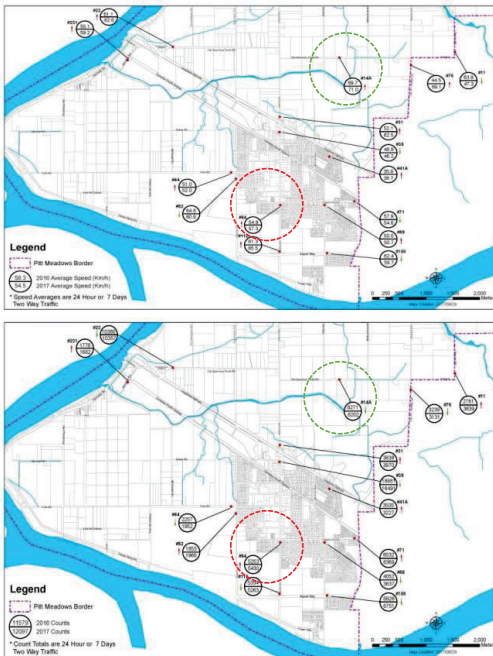
PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 10





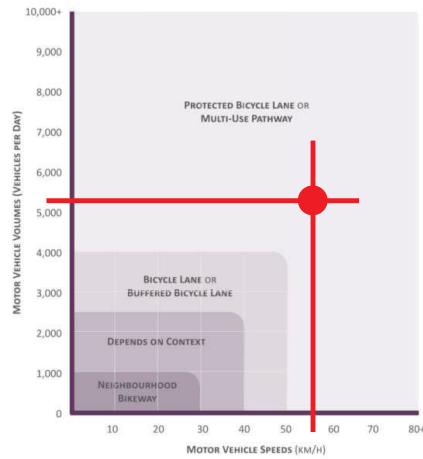
- DEVELOP REPORT TEMPLATE
- DOCUMENT COMMUNITY PROFILE (I.E., DEMOGRAPHICS AND TRAVEL PATTERNS)
- DOCUMENT SITE VISIT FINDINGS
- DEVELOP EXISTING CONDITIONS MAPS FOR PEDESTRIAN FACILITIES AND CYCLIST FACILITIES BY LEVEL OF COMFORT

• TRAFFIC DATA TO INFORM FACILITY TYPE SELECTION



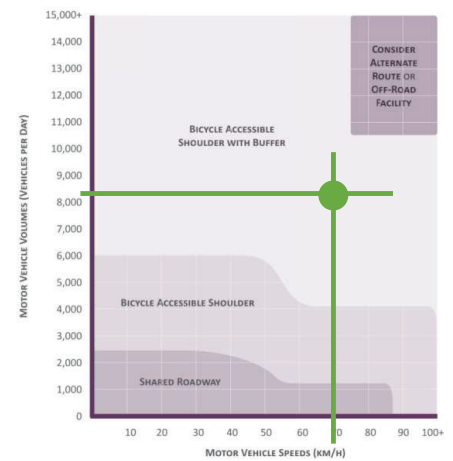
Example: Harris Road

BICYCLE FACILITY SELECTION DECISION SUPPORT TOOL
URBAN / SUBURBAN / DEVELOPED RURAL CORE CONTEXT



Example: Dewdney Trunk Road

BICYCLE FACILITY SELECTION DECISION SUPPORT TOOL
RURAL CONTEXT



NEXT STEPS – DATA COLLECTION

PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 13



• PUBLIC ENGAGEMENT - ISSUES AND IDEAS SURVEY PART 1

• DEMOGRAPHICS:

- AGE
- LOCATION
- GENDER
- HOUSEHOLD
- DISABILITIES
- CARS IN HOUSEHOLD
- BIKES IN HOUSEHOLD
- OTHER MICRO-MOBILITY



NEXT STEPS - ENGAGEMENT

PITT MEADOWS | ACTIVE TRANSPORTATION PLAN | SLIDE 14



- **PUBLIC ENGAGEMENT - ISSUES AND IDEAS SURVEY PART 2**

- TRAVEL PATTERNS:
 - TRIP FREQUENCY BY MODE
 - AT TRIP PURPOSE
 - REASONS FOR USING AT
 - REASONS FOR NOT USING AT
 - INTEREST IN TRANSFERRING TRIPS TO AT
 - LEVEL OF CONFIDENCE AS A CYCLIST

- **PUBLIC ENGAGEMENT - ISSUES AND IDEAS SURVEY PART 3**

- HEALTH AND EQUITY:
 - IMPORTANCE OF AT FOR HEALTH
 - FEELING OF SAFETY BY MODE
 - WHAT CONTRIBUTES TO SAFETY CONCERNS
 - TRANSPORTATION BURDENS – I.E., TIED TO A MODE FOR SPECIFIC REASONS
 - EXTENT TO WHICH EXISTING NETWORK LIMITS TRANSPORTATION CHOICE

- **PUBLIC ENGAGEMENT - ISSUES AND IDEAS SURVEY PART 4**

- INFRASTRUCTURE:

- LEVEL OF COMFORT WITH DIFFERENT FACILITY TYPES, I.E., PAINTED BIKE LANE, MULTI-USE PATHWAY, PROTECTED BIKE LANE, UNPAVED TRAIL, ETC
- IMPORTANCE OF STREET DESIGN ELEMENTS, I.E., SEPARATION FROM VEHICLES, TRAFFIC CALMING, STREET LIGHTING, BICYCLE PARKING, WAYFINDING

- MAP FUNCTION:

- FOR LOCATION SPECIFIC ISSUES

- IDEAS FUNCTION:

- SHARE IDEAS WITH THE COMMUNITY



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Pitt Meadows Road and Rail Improvements Project
Update to Active Transportation Advisory Committee

Raul Armendariz-Rodriguez, senior project delivery specialist,
port authority

April 11, 2022



Presentation overview

Agenda

Introductions

Project update

Current state versus proposed upgrades


- Current conditions

- Harris Road underpass

Next steps

Questions and discussion

Note: The design for the Harris Road Underpass continues to be progressed. Any images shared today are indicative and for discussion purposes only, as they are subject to change through further consultation with the City, project partners, other stakeholders and rights holders.



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Pitt Meadows Road and Rail Improvements

The project

Vancouver Fraser Port Authority

In partnership with the City of Pitt Meadows and CP, we are leading the following road improvements:



A new four-lane underpass beneath the Canadian Pacific (CP) rail tracks at Harris Road



A new two-lane overpass above the CP rail tracks at Kennedy Road



Canadian Pacific (CP)

In parallel, CP is leading the following rail improvements:



A new siding track on the north side of the existing tracks between Harris Road and Kennedy Road



An extension of the existing lead track to the Vancouver Intermodal Facility east across Harris Road

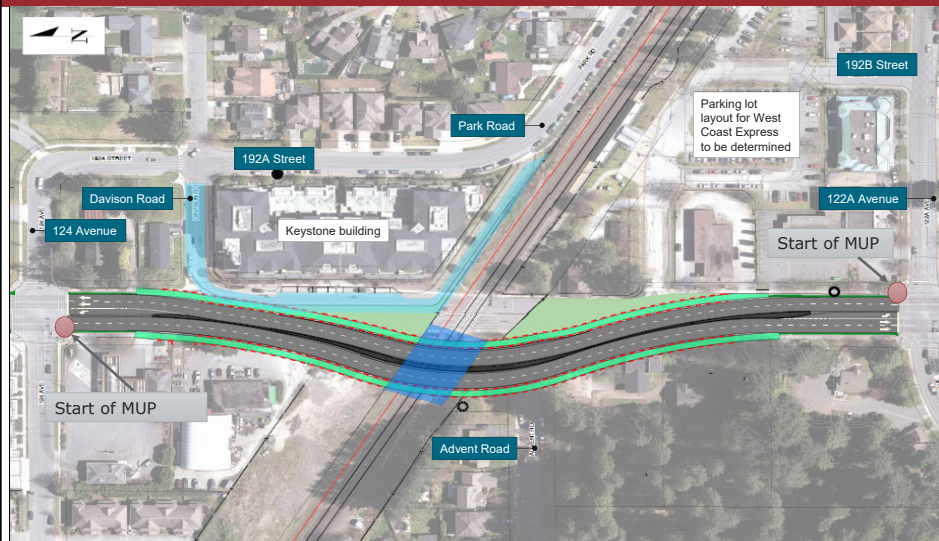


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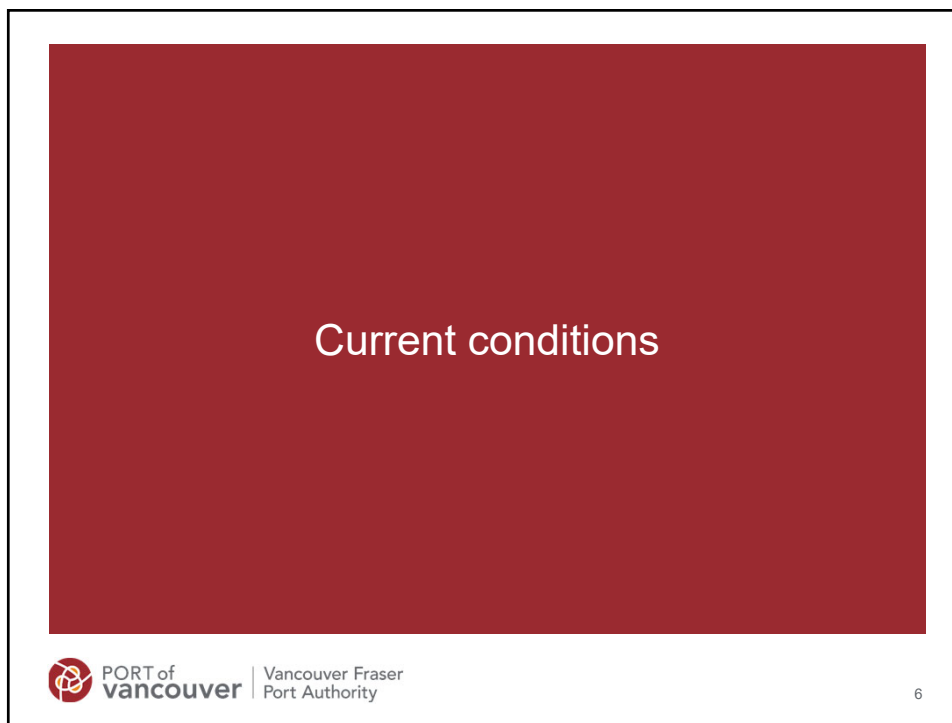
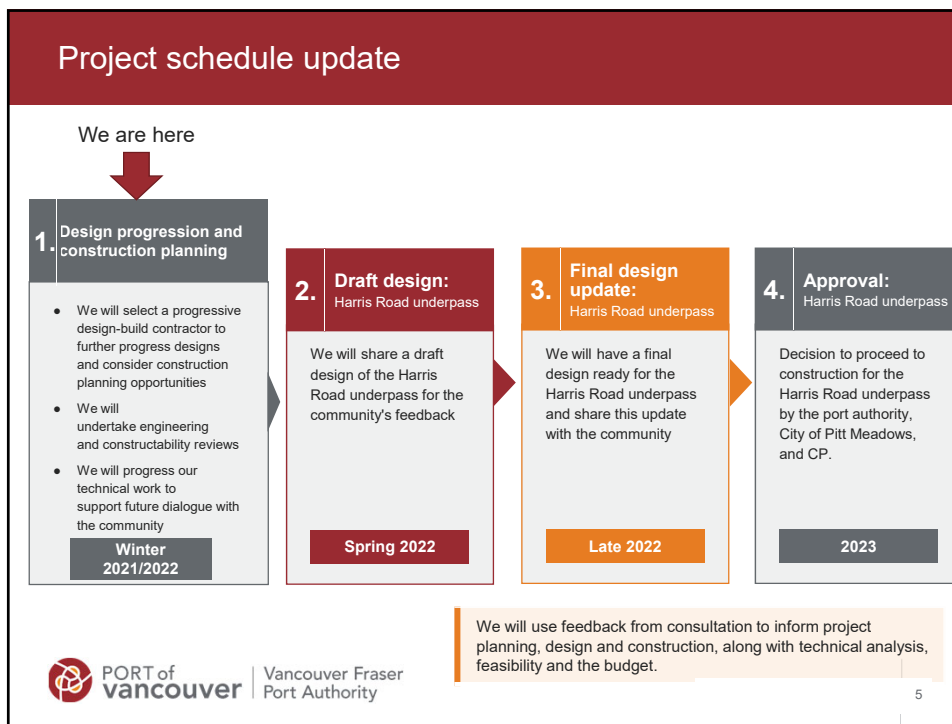
Harris Road underpass – Updated alignment



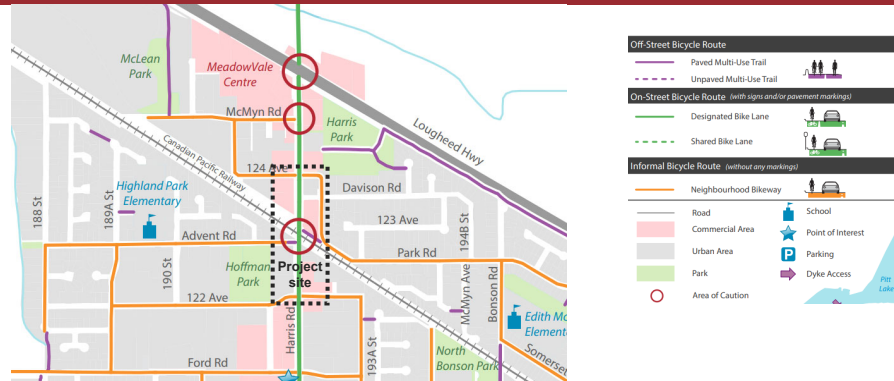
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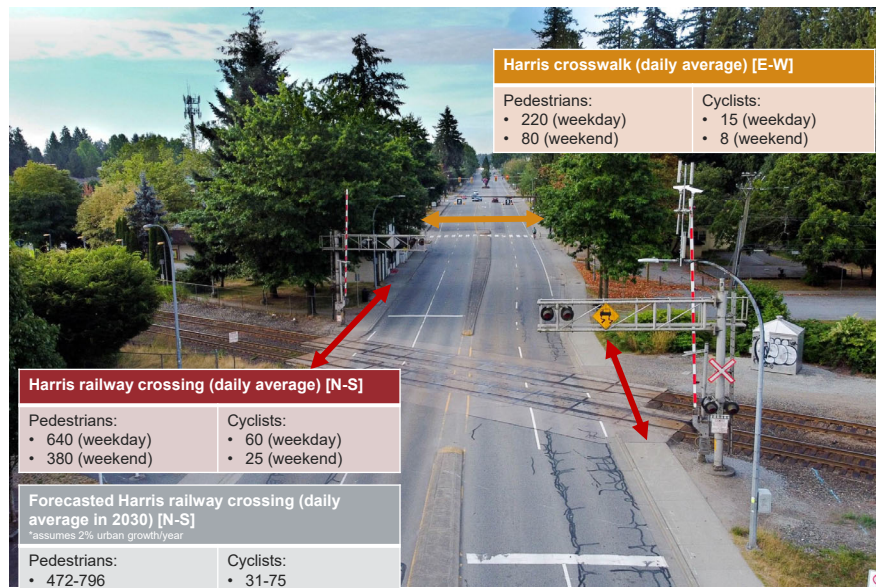
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Cycling network



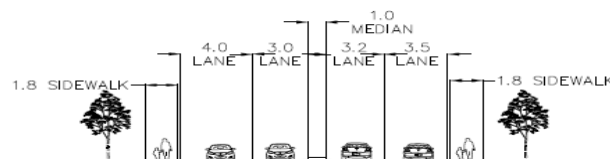
- City of Pitt Meadows Cycling Map indicates a designated bike lane (on-street bicycle route) along Harris Road
- "Area of Caution" indicated at Harris Road railway crossing
- Adjacent roads have short off-street facilities or neighbourhood bikeways



At railway crossing – Current conditions



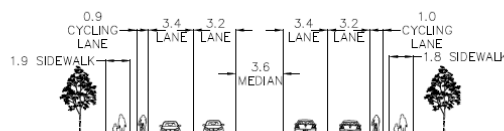
Note: no cycling facilities at railway crossing



South of crossing – Current conditions



Note: narrow cycling shoulders/lane



Proposed upgrades



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What we've heard to date (HUB Cycling and ATAC) – 1/2

- Minimum clear MUP width of 3.5m including under rail structure (previously requested a 4m minimum)
- Appropriate grade % based on transportation guidelines and mobility assistance
- Ensuring safe and welcoming environment for all active transportation mode users of all ages and abilities, regardless of their mode of travel
- Improve the walking, wheeling and cycling experience with MUP access and connectivity, including connectivity to Keystone, WCE, and Museum area
- Facilities built on both sides of the underpass with users separated from vehicles
- Suggested shared elevated pathways to keep cyclists off the road
- Preference for bike travel to be uni-directional only



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What we've heard to date (HUB Cycling and ATAC) – 2/2

- Consideration of visual aspects such as bright colors, public art, active edges and lighting to create inviting atmosphere
- Delineation between cycling and other MUP modes via colour/line/surface finish material
- Utility infrastructure integrated into design so MUP is free from obstructions
- Accommodate mobility/wheelchairs
- Consideration of speed differentials for rapidly evolving transportation including micro-mobility options (e-scooters, electric bikes, bike share etc.)
- Appropriate ceiling clearance for users
- Consideration of noise mitigation for underpass users including sound absorbing materials
- Consideration of ventilation requirements for the underpass
- Consideration of transition points and design

Design standards

City of Pitt Meadows bylaw

- Maximum walkway longitudinal grade of 7% desirable and 9% absolute

TAC 2017

- Allows vertical grades up to 12%
- Indicates grades over 6% considered steep for cyclists

BC Active Transportation Design Guidelines

- 5% maximum grade is desirable
- 8% maximum grade is defined as ultimate maximum

Design decisions and proposals

Decisions we have made and/or are proposed in our design:

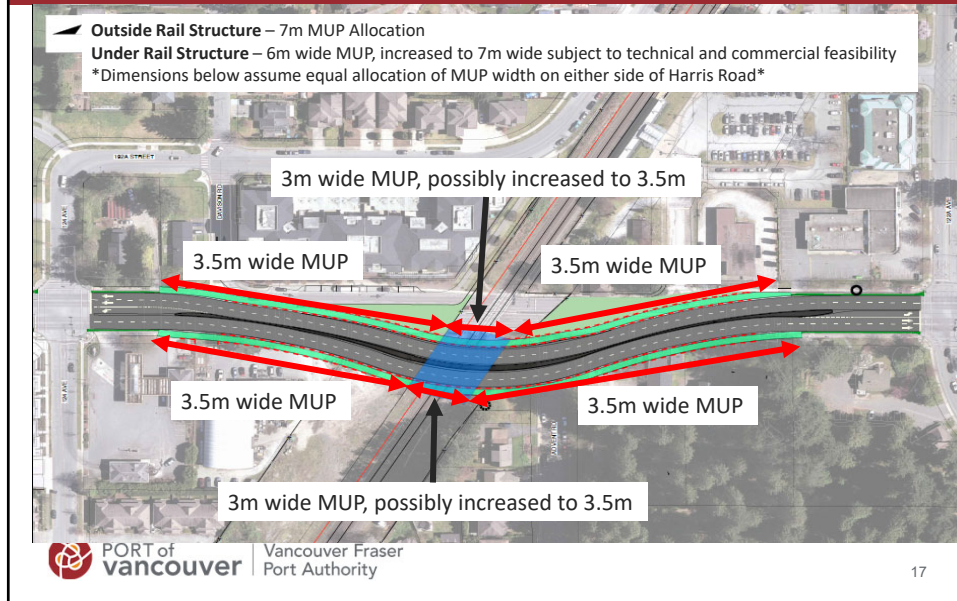
- MUP minimum widths (as per PPA)
- Maximum grades (as per PPA)
- Uni-directional vs bi-directional
- MUP length, access and connectivity
- Surface treatment
- Location of active transportation users on MUP
- Transition to existing facilities
- Signage and pavement markings
- Lighting
- Public realm opportunities

Project partnering agreement – Commitments (active transportation related)

The following key design parameters for the permanent infrastructure have been agreed by the parties, subject to overall Project affordability:

- (a) The road will have a 100 year design service life and will be designed to accommodate four vehicle lanes (two lanes in each direction) and with dedicated space for pedestrians and cyclists as outlined in Section 5.1(c) and Section 5.1(d).
- (b) A maximum grade of 5% for pedestrians and cyclists and 9% for vehicles, which will meet TAC design and BC Active Transportation design guideline recommendations.
- (c) North and south of the rail bridge structure and within the project limits, dedicated spaces will be provided for the accommodation of cyclists and pedestrians on both sides of the roadway (such as a multi-use path(s) or sidewalk(s)), with a combined and unencumbered total width of 7.0 meters (excluding foundation bases for street lighting).
- (d) At the rail bridge location, dedicated spaces will be provided for the accommodation of cyclists and pedestrians on both sides of the roadway (such as a multi-use path(s) or sidewalk(s)), with a combined and unencumbered minimum total width of 6.0 meters. Where technically and commercially feasible, this width will be increased to a maximum width of up to 7.0 meters.

Project partnering agreement – Commitments (MUP conceptual description, City of Pitt Meadows)



Contractor design specifications (active transportation related) – surface material

- (I) All multi-use paths shall be surfaced with a minimum 50 mm thick asphalt layer or a minimum 100 mm thick concrete layer, underlain by compacted well-graded crush granular base in not less than 150 mm thickness, underlain by sub-base material as per the design recommendations of the Design Builder's geotechnical engineer. All granular materials shall be constructed as per the MMCD. All gravel surfaces to be asphalt surfaced shall have an emulsified asphalt primer.

For active user comfort, we are proposing asphalt surface on the MUP

Active Transportation Guidelines:

- As multi-use pathways are intended to be accessible and accommodate a wide range of users and trip purposes, **asphalt is the preferred surface type**. Asphalt surface treatment provides a smooth continuous surface that is accessible for all user groups
- The preferred material for a bicycle pathway is asphalt.
- To ensure the pedestrian facility is accessible and can accommodate a variety of users, the preferred pathway material is asphalt or concrete.

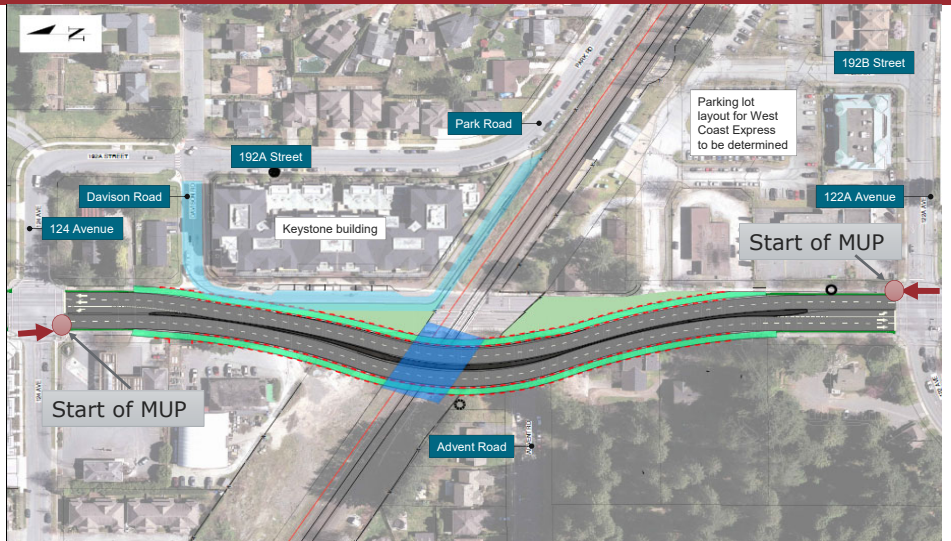


Contractor design specifications (active transportation related)
– surface material

- Surface materials considered:

Asphalt	Concrete	Pavers
<ul style="list-style-type: none"> • Provides continuous, smooth, joint-free travel • Quieter with improved reliability preferred for wheel transportation modes (cycling, rollerblading, strollers etc.) • Easier on user joints • Softer aesthetic look • Requires regular maintenance and sealing 	<ul style="list-style-type: none"> • Longevity • Minimal maintenance requirements • Most frequently used MUP material for local grade separations • Harsher aesthetic look • Expensive installation • Harder on joints of users 	<ul style="list-style-type: none"> • Versatility of shapes and patterns • Can replace individual pavers • Good traction when wet • Weeds can grow in cracks • Can move from roots, frost and settling • Safety concerns (tripping) • Can be difficult for wheel transportation modes

Cycling direction – one way



Cycling direction – one way

Why is uni-directional proposed?

- Tie into existing conditions
- Reduces conflict points
- Constraints and challenges with contraflow travel if bi-directional
- MUP width constraints
- Increased user safety

Active Transportation Guidelines:

- Uni-directional pathways travel in the same direction as motor vehicle traffic and **also provide greater access to destinations than a bi-directional multi-use or bicycle pathway on one side of the road**
- Uni-directional protected bicycle lanes in the direction of motorized travel are generally the preferred option to integrate bicycle facilities into the existing operation of the road. **This configuration can simplify movements at intersections and provides intuitive and direct connections with the surrounding transportation network**, including similar transitions to existing bicycle lanes and shared travel lanes.



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Example of transitions to MUP



For discussion:

➤ Separation of cyclists and pedestrians

- Signage
- Pavement markings
- Center line

Active Transportation Guidelines:

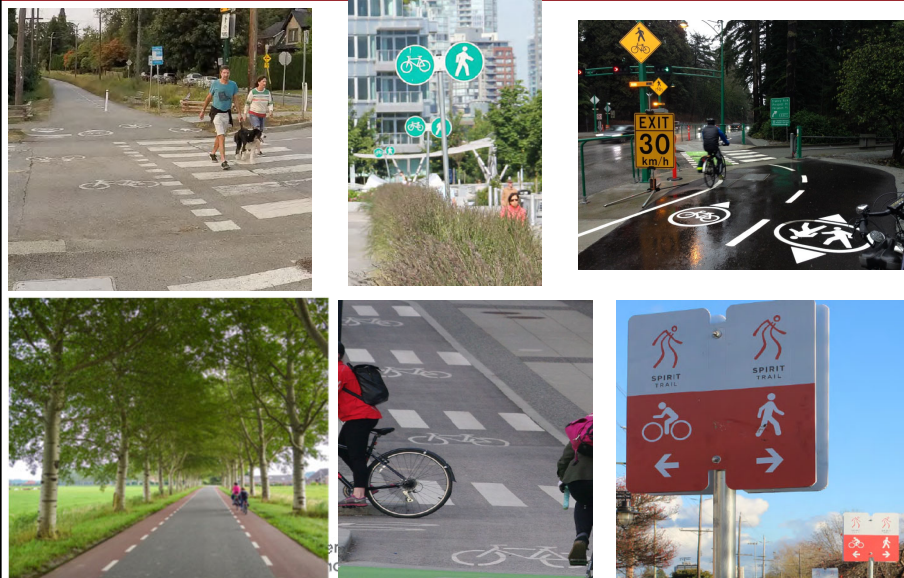
- **Separation can involve anything from painted lines to physical separation.**
- Pavement markings providing guidance for types of users and direction of travel
- Centreline striping is recommended when multi-use pathways are located on hills with a grade steeper than 5%, at **locations where passing is dangerous due to space constraints and limited visibility**



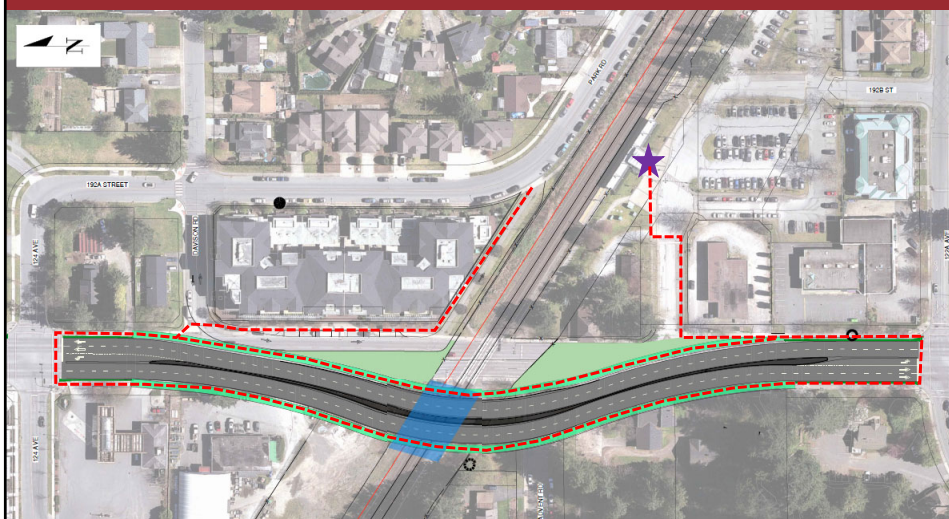
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Example of separation – for discussion



MUP access



Contractor design specifications (active transportation related) – Signage and amenities

11.3.6 Cyclist Signage/Pavement Marking

Signage shall provide positive guidance where required and wayfinding over the Project area. The following destinations, as a minimum, shall be included in the wayfinding signage:

- (a) Harris Road;
- (b) West Coast Express station;
- (c) Heritage Buildings;
- (d) Advent Road;
- (e) Davison Road;
- (f) Keystone frontage lane;

- (i) All pedestrian, cyclist and multi-use path routes shall be provided with full continuous lighting. Lighting levels shall meet or exceed the design criteria outlined in the City's standards.

- (j) The design of MUP lighting within the underpass shall consider the need for daytime lighting in the event of an elevated MUP structure away from the roadway and for mix of cyclists and pedestrians sharing a common pathway.

11.3.3 Ramp/Road Crossings

- (a) Multi-use paths shall be direct, safe and convenient. The use of spiral ramps and switchbacks shall be avoided, where possible.
- (b) Where pedestrian and cycling facilities are to be provided, such as crosswalks, safety elements shall be included to improve driver awareness of crossing pedestrians and cyclists. Such elements shall include additional signage, lighting and pavement markings.



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Examples of signage and amenities – for discussion



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Examples of lighting – for discussion

Active Transportation Guidelines:

- Ample lighting is important to ensure adequate visibility between all shared users.
- Where feasible, lighting should be provided **along the entire active transportation facility.**
- Lighting should also **be provided outside of the underpass entrances at night** so that the contrast between the interior of the underpass and the exterior does not cause visibility challenges



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Contractor design specifications (active transportation related) – connections

11.4 Specific Requirements

This Article describes the requirements for the Design that shall be provided for cycling and pedestrian facilities in specific areas.

11.4.1 Harris Road

- (a) Design speed of 35km/h for cyclists is required for multi-use paths

PART 294 The existing sidewalk on east side of Harris Road shall remain connected to the existing WCE parking lot. Pedestrian and cyclist access to WCE parking lot off Harris Road is to be retained.

PART 295 The new MUP shall be connected to the existing bike lane and sidewalk on both sides of Harris Road.

PART 296 A new sidewalk shall be constructed along the south side of Keystone building to provide pedestrian walkway connectivity between Harris Rd and 192A St.

Active Transportation Guidelines:

- The design speed of a multi-use pathway should be able to accommodate the preferred speed of the fastest pathway users, while also considering the need to control speeds in a multi-use setting.
- Electric bicycles can provide power speeds assist up to a maximum of 32 km/h
- Using a design speed of 30 km/h is an appropriate speed in most contexts.

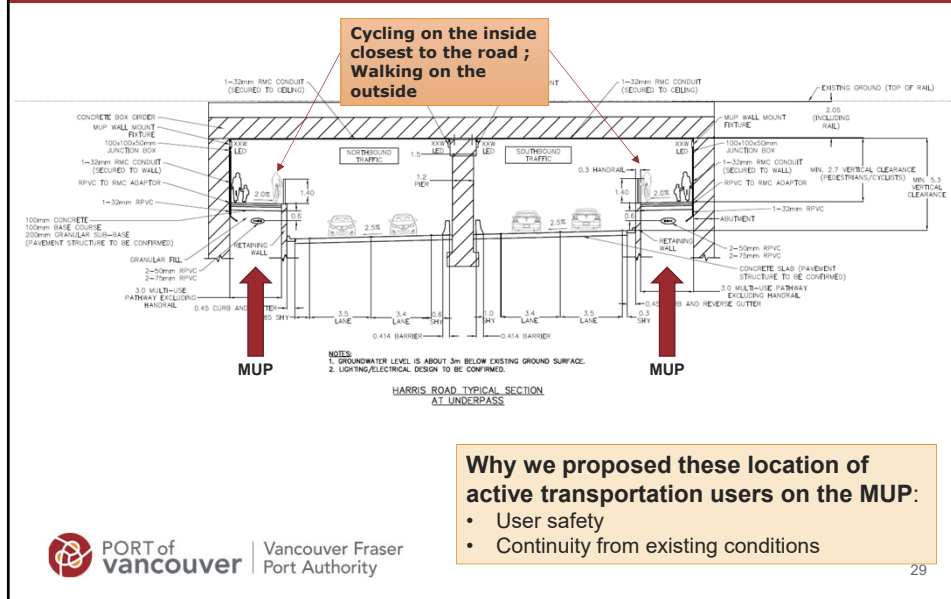


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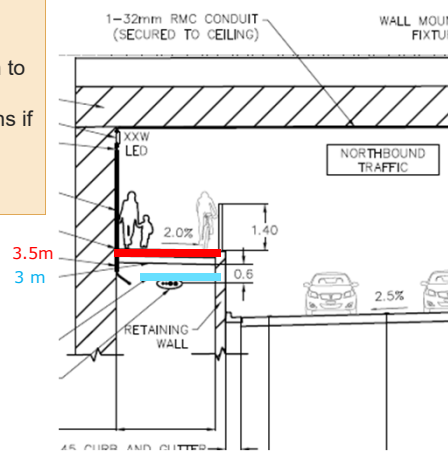
Harris Road underpass – Technical cross-section



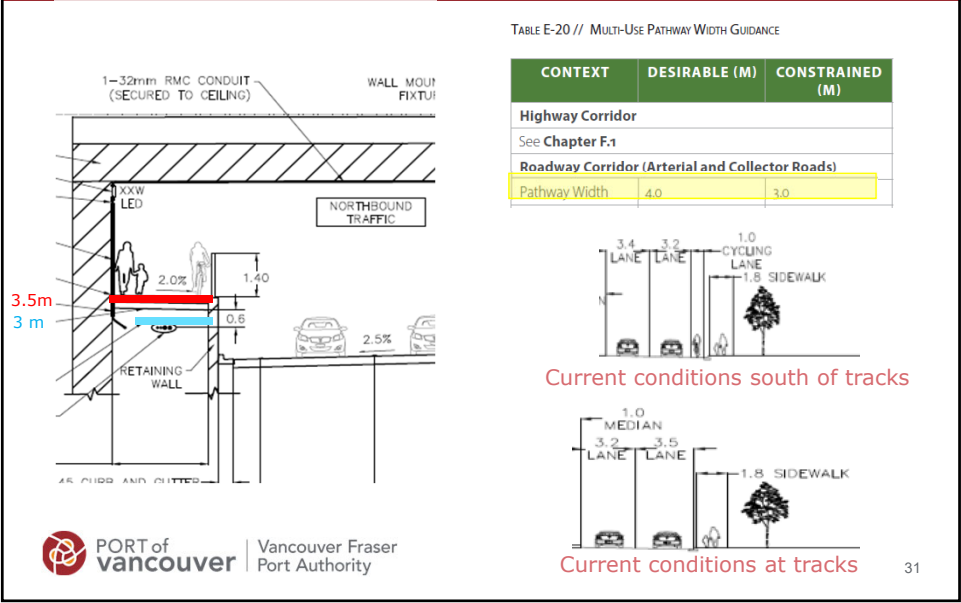
Harris Road underpass – Technical cross-section

ATAC preference for consistent bike width throughout MUP.

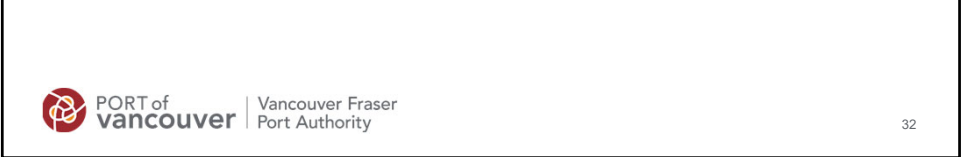
- What is the right ratio of pedestrian to cyclist space?
- Is there a pinch point for pedestrians if consistent width is maintained ?
- Consideration of mobility aid width when determining minimum ratios



Harris Road underpass – Technical cross-sections



Harris Road underpass – North of railway looking south

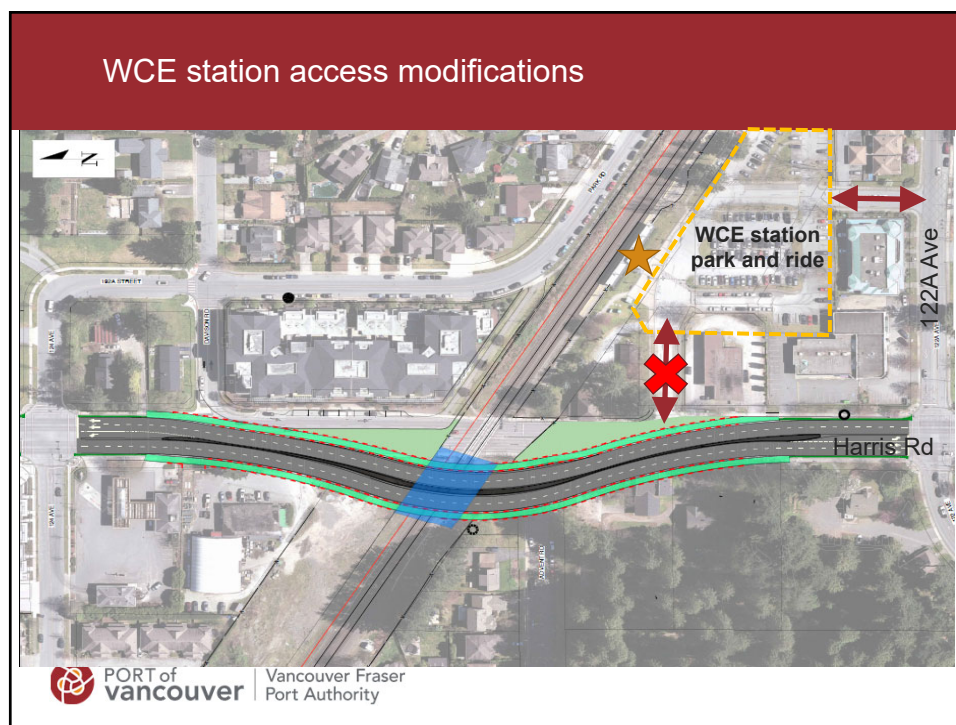
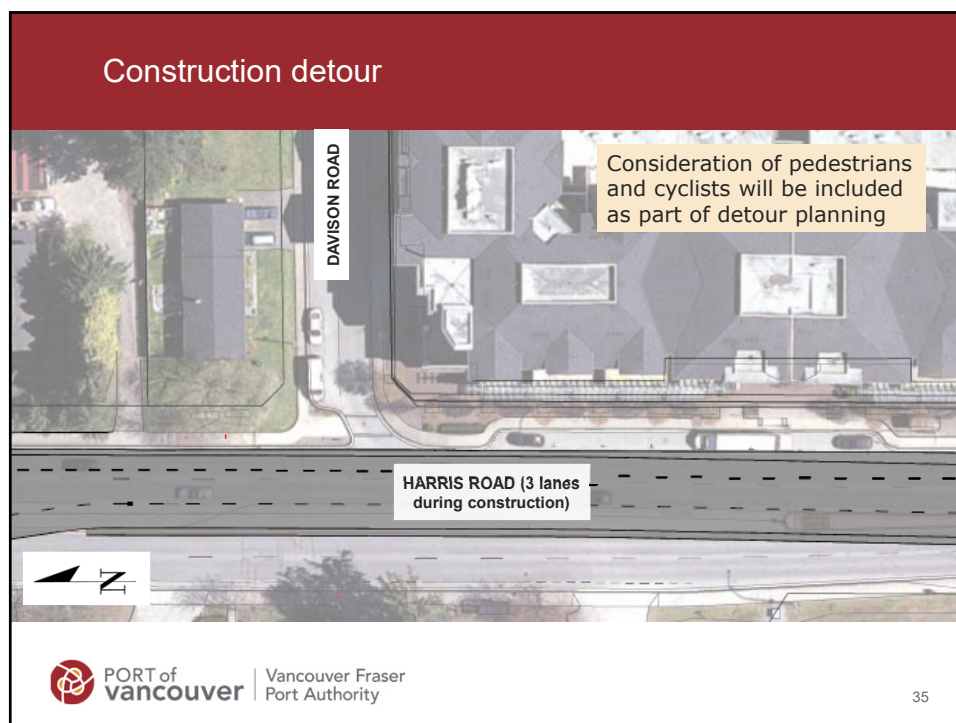


Harris Road underpass – South of railway looking north



Harris Road underpass – Cross-section through underpass





Next steps

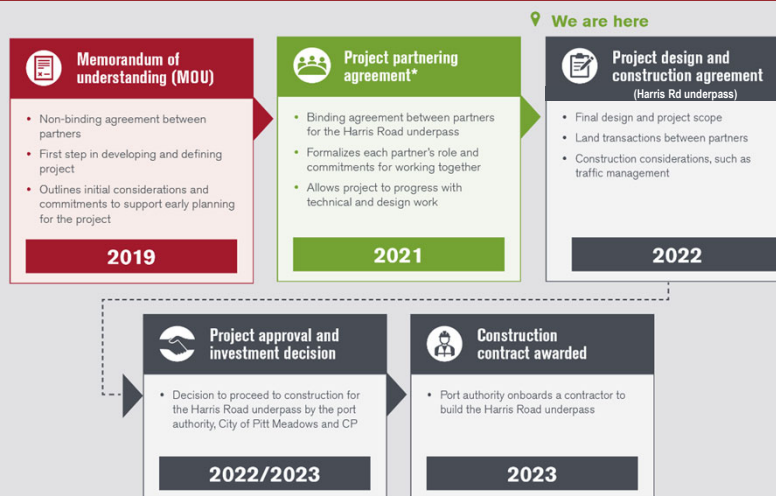


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Harris Road underpass – Approval timeline*



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*Please refer to the fall update Q&A for more information (see slide 35)

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Community updates and engagement in 2022

We have planned the following community updates and engagement activities in spring 2022 and encourage your participation.

Update: Noise and vibration study

- Updates to the noise and vibration study, mitigation considerations and next steps

Engagement: Harris Road underpass

- Progressive design-build contractor involvement
- Updates to roadway design, active transportation, transit
- Heritage building relocation update
- Public space considerations
- Construction considerations
- Other project updates

Update: Rail scope

- Update on CP's rail scope (progress of lead and siding track)



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Pitt Meadows Phase 2B | Spring Engagement 39









Discussion



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Harris Road underpass - What we heard from community		
Considerations		Action
	Maintaining access to Harris Road during construction	✓
	Keeping West Coast Express operations open during construction	✓
	Maintaining business access	✓
	Opportunities to improve public realm/urban space	✓
	Respecting archeologically sensitive sites for Indigenous groups	✓
	Enhancing safety for active transportation and pedestrians	✓
	Considering noise and vibration mitigation related to the project	✓
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What this means – Technical considerations



- Allowing sufficient space for traffic management during construction
- Maintaining access to businesses during and after construction
- Ensuring fire access requirements are maintained to adjacent buildings
- Providing appropriate grades for driving, cycling, and pedestrians
- Identifying opportunities for new public spaces, urban design and cultural recognition
- Applying other relevant technical design requirements – for example, delineating underpass structure from adjacent private structures



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What this means – Considerations for the road alignment



- Maintaining the existing straight north-south alignment of Harris Road is not possible – assuming we continue to prioritize:
 - Keeping Harris Road generally open to traffic during construction
 - Providing businesses with access during and after construction
 - Minimizing the number of private properties impacted
- =
- Solution: shift Harris Road slightly west at the current railway crossing

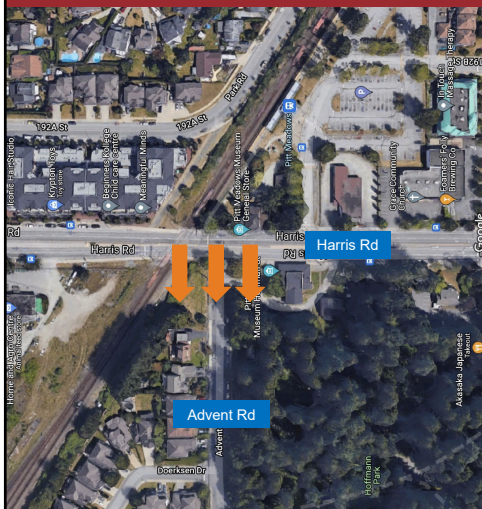


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Harris Road underpass – Updated alignment



- ✓ Keeps Harris Road generally open to traffic during construction
- ✓ Provides businesses with access during and after construction
- ✓ Minimizes the number of private properties impacted

ATAC - Annual Project Priorities Summary

Item	Project	Priority	Status	Location	Est. Cost 2022	Possible Grants 2022		Est. Cost Future Years
29	Curb Letdown	High	Cannot grind, too steep. Submitted grant application	Wildwood Crescent & North Wildwood Place Southwest, expanded to include wildcrest and select sidewalk improvements	\$16,000	\$16,000	WITT	
30	Curb Letdown	High	Cannot grind, too steep.	Wildwood Crescent & South Wildwood Place Northwest	\$7,500		N/A	
32	Multi-Use Path	Medium	Prelim Design complete. Collect data at this location as part of Bike Network Review and then revisit for 2023. Currently 'on hold' in business plan.	193rd complete street concept design, traffic data collection				
33	LPI/Advanced Green	High	Grant obtained for LPI and few other items on Harris Rd/PME	Hammond Road & Bonson Road (Allowance only), N/S leg and signal coordination	\$500	\$500	Vision 0	
37	Education	High	Ride the Road course for 2 schools in 2022. PME confirmed for end of May and HUB still planning to coordinate with 1 more school.	HUB - Bike to School Education - Highland Park, Edith McDermott, Davie Jones and Pitt Meadows Elementary	\$9,900			\$5,500
40	Bike Network Review	High	Scope expanded to include assessment of network gaps, lack of crossing facilities, wayfinding, inaccessibility and consultation. Grant received (50%) and work planned to start late Q1 2022 and data collection during the summer. Collaborate with Parks for additional data collection locations.	Review of City's Bike Network (\$10,000 data collection, ~20 sites), plus expanded assessment	\$26,013	\$26,013	BC Active	
44	Neighborhood Bikeway	Low	Committee to revisit extension of bikeway following Bike Network Review and data collection (~2023)	Extend neighborhood bikeway along Blakely Rd (north of 120B Ave), 121B Ave and connect to 193 St				TBD
48	MUP on Kennedy Rd	TBD	Committee to revisit in future years to provide connectivity from south end of Kennedy overpass to dike	Potential MUP from Katzie Slough to Kennedy Rd pump station on north side of the road				TBD
55	Leading Pedestrian Intervals	High	Grant obtained for LPI and few other items for Harris Rd/PME	McMyn/Harris Rd and 122/Harris Rd based on recent TOSR recommendations (plus audibles at McMyn, not part of grant)	\$2,500	\$1,000	Vision 0	
56	Crosswalks	Medium	Potentially for implementation in 2023 - revisit scope (curb extensions, relocation of parks trail, transit stop, etc.)	122nd Ave and 191B St intersection - review and relocate to west leg				\$5,000
57	Lougheed Trail	Medium	Potential localized repair in 2022 in coordination with Parks Department - work pushed to 2023 due to budget	Repair localized section along the trail, with specific roots/bumps west of the Katzie Slough			N/A	\$5,000
58	Letdowns - Somerset/Chestnut	High	Area assessed and quote to be obtained in ealy 2022.	Various rollover curbs with no real letdowns to be ground down and one letdown to be relocated	\$10,000		N/A	
59	Accessibility Improvements	High	Review planned for Q2 of 2022	Accessibiltiy improvements at various dike/trail entrances, such as parking and pole relocation	\$10,000			
60	Bikeway Markings	Medium	Potential for design/implementation in 2022	Markings and signage between Harris Rd and Blakely Rd on 119 Ave	\$10,500	\$10,500	BICCS	
61	Trail Etiquette Signage	Medium	Potential design/implementation of some trail/dike signage in collaboration with Parks - push implementation to 2023	Various trails/dikes				\$5,000
62	Wildwood - south crosswalk	Medium	Potential for design/implementation in 2022	Crosswalk (with elephant feet), letdowns and Wildwood Trail access improvements	\$7,875	\$23,625	BICCS	
Estimated Total					\$100,788	\$77,638		\$21,000
Active Transportation Annual Budget					\$100,000			\$100,000

Possible Grants (Applications submitted)

Complete

Revisions to status

Postponed

Newly Added

DM 162753v6

Active Transportation Advisory Committee - Action Item Tracker

Meeting Date	Action Item	Status	Comments	Assigned To:
11-Apr-22	Lougheed Trail - clean up	In Progress	Staff to follow-up with Parks Department on clean up of Lougheed Trail surface, specifically at west end near Kennedy Rd and vegetation overgrowth.	A. Torres
07-Feb-22	Potential Parking Removal at NE corner of McMyn/191 St intersection	Complete	Staff reviewed sightlines and guidelines and removal of parking is not necessary	A. Seed
	AAA Cycling connections - GEBP, Parkside Trail and town centre	Revisit in June 2022	Review and also future Harris Rd - feasibility study for active transportation improvements (possible DCC project)	S. Maki
08-Nov-21	Sidewalk sloped along south side of Park Rd	In Progress	Parks/Operations staff have reviewed the area and prioritized trees for removal, sidewalk panel replacement and locations to monitor/revisit in 2023	A. Torres
	ICBC coverage for damaged bollards	Complete	Staff submitted reimbursement of bollard damage to ICBC	A. Torres
13-Sep-21	Airport Way/Southgate Rd - Pushbutton Relocation	Revisit in late 2022	Staff to include as part of Airport Way widening design. Design ongoing in 2022 and implementation 2023/24	S. Maki
	Trail/Dike Etiquette Signage	Revisit in Q3 2022	Staff discussed with Parks and Metro Vancouver. Updated dike signage (not new) is planned for 2022 as part of Parks workplan and we have requested review of draft content (likely mid-2022) and can likely include additional information about e-scooters, user etiquette and wildlife. Possible QR code inclusion - Staff to follow-up with Emergency Management and Parks.	A. Torres
	Awareness Campaign Improvements/Expansion	Implement May 2022	Staff to discuss inclusion of action PM images, more pedestrian information and wildlife information (coyotes) with Communications Department. Strategy to be	A. Torres
	Ford Rd, West of Baynes: Connectivity Improvement	In Progress	Staff discussed with the Airport. They had interest, but think it is something to be further explored once more development/build out occurs. Staff will also look into DCC eligibility as part the 2022 DCC Program review. Ped/Cyclist data to be obtained in this location as part of Active Transportation Grant/Network Review	S. Maki
14-Jun-21	Kennedy Rd Overpass	Revisit in mid 2022	Committee to further discuss/provide feedback once project component is revisited.	S. Maki
	E-Scooter/E-Bike Awareness	Revisit in late 2022	Committee to revisit as part of future priorities. Not a focus for 2022, but revisit as part of 2023 workplan.	A. Seed
	Accessibility Improvements at Various Dike and Trail Entrance Locations	In Progress	Staff to review locations of improvements, such as designated parking and pole relocation (4ft clearance). Likely prioritized for review of areas in Q2 2022	A. Seed
12-Apr-21	Staff to review width of pathway connection into Lasser Rd. to ensure accessible for cyclists and pedestrians.	In Progress	Staff have re-iterated the need for a wide connection (4m) and await detailed drawing submission and will review further at that time.	S. Maki

Active Transportation Advisory Committee - Action Item Tracker

09-Nov-20	Request for additional information on Lougheed highway improvements at Harris Rd, especially in relation to rapid	Revisit in mid-2022	High-level discussions ongoing with MOTI and staff will bring forward more information, once available.	S. Maki
08-Jun-20	Staff to look up previous motion to council regarding a recommendation that the ATAC had made a to Council regarding reducing the speed limit on City side streets from 50 kms/hr to 30 kms/hr.	Revisit prior to next master plan update	Resolution went to UBCM in 2019 and was endorsed. RCMP rep. generally supports, but 40 km/hr (not 30km/hr). MOTI has been asked to consider modifying the Motor Vehicle Act in order to allow incorporated municipalities to institute blanket speed zones in residential areas. Staff discussion at Nov 9th meeting and Vision Zero link included for Committees review. https://visionzero.ca/ . Further consideration for inclusion in next update to master plan.	S. Maki

ATAC 2022 Work Plan	
February 07	Status
• Parks and Rec Master Plan Update – Committee Formation	Complete
• 2022 Committee Recruitment	Complete
April 11	
• Harris Rd Underpass – Feedback on Active Transportation Design	Ongoing
• Statistics Update	Pushed to June
• TransLink Grant Funding Approval Update	Pushed to June
• Active Transportation Network Review – Kick-off & Data Collection Locations	Complete
• ATAC EPC Draft Presentation – Committee Feedback	Ongoing
June 13	
• Budget Review and Expenditure Summary Update	
• HUB – 2022 Cycling Education Program Update	
• Committee Meeting Format	
September 12	
• Review of 2023 Priorities and Budget Allocation	
• Project Review for Grant Funding Applications	
• Active Transportation Network Review – Update	
November 07	
• 2023 Business Planning Presentation (late November)	